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&  
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&  
2017Moto**

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## **Generalities**

UKOPRA is the governing authority for offshore powerboat racing within the UK. That mandate is given by the licenced competitors and also the following signatory race organisers:

British Powerboat Racing Club

International Powerboat Racing Club

2017Moto

\*valid at 31/1/18

By the taking of a licence for competition within UKOPRA sanctioned races the competitor is deemed to be a voting member of the association and a party to the creation of the rules herein.

By acceptance of UKOPRA sanctioning an event, the event organizer agrees to acceptance of the rules herein. Any officers appointed to the race under these rules are deemed to be a voting member of the association and a party to the creation of the rules herein.

Any member of the association having concern over any rules should lodge that concern by email to [info@ukopra.co.uk](mailto:info@ukopra.co.uk) in order that it may be considered.

## **Precedence of rules**

All members are advised that the rules are specifically for the purposes of fair competition. Any safety rules are made purely as a minimum standard to be applied, both the competitor and event organiser recognise that their own risk assessment and application of additional recommendations is to be carried out prior to any competition.

For clarity the laws pertaining to Safety of Life at Sea (SOLAS) and The International Regulations for Preventing Collisions at Sea (COLREGS) take precedence at all times over any rule herein or race instruction issued by the organiser.

Similarly, any Local Legislation as published by Harbour, Foreshore or similar authorities will take precedence over any rule herein or race instruction issued by the organiser.

In recognition should a competitor suffer a competitive loss from adhering to the above application may be made for fair adjustment to their final position as restitution.

Rules herein may be amended for local conditions by race instruction and in that case will take precedence.

## **Racing Values**

Persons racing under these rules agree that discrimination on any grounds are unacceptable in the sport of powerboat racing. They will encourage and welcome the participation of people from all nations, religions and physical ability or differences and creeds without prejudice.

In all competitions and meetings, a fundamental consideration shall be honesty and openness. Each meeting and individual participating shall adopt zero tolerance policy towards alcohol, drug abuse, doping or other enhancements.

For clarity the standards applied will be as those accepted on the public highway and both competitors and race officials will submit themselves on request to medical examination by the medical officer

All boats, competitors and officials participating shall adopt zero tolerance policy towards any modifications or adaptations that break the rules and regulations of the rulebook. Competitors who are found to knowingly cheat by means of false representations or mechanical or other adaptations outside of the parameters of the rule book shall face an automatic ban of no less than 1 season.

All officials shall be prepared to demonstrate fairness and impartial judgment if necessary.

All persons connected with powerboat racing shall at all times discharge their duties

to the best of their abilities without causing harm to any participants, spectators, officials or any other person.

All persons connected with powerboat racing shall declare any conflict of interest and excuse themselves from any function that may affect competition results.

For the purposes of prevention of conflicts of interest, no officials or other persons shall be permitted to operate any position of authority related to points scoring, jury, scrutineering or any other meaningful sporting role if they have a family, marital, relationship or business connection with the competitors under that function.

### **General Conduct**

All participants resolve to hold the sport in highest regard and to the standard of their surrounding peers.

In any matter of serious disregard or major sporting conflagration the member or event organiser shall submit to a tribunal formed of four members of the association. Objection to any member taking the position will be accepted and replaced by an alternative member.

Being a jury of peers any judgment handed down from that tribunal is to be final and not subject to appeal.

Any matter simply relating to competition is to be managed by the protest procedure and race jury at the event. Race jury decisions are considered to be final and binding except for the provisions above.

### **ENVIRONMENTAL**

Competitors and race organisers are at all times to respect the environment and manage their activities to avoid unnecessary pollution or nuisance.

## **101 - EVENT SANCTIONS**

Organisers wishing to hold an event utilising the licencing, rules and administrative should request to calendar their event and sanction will not be unreasonably withheld.

### **101.02 - OTHER MEETINGS**

It is to be noted that it benefits all organisations if the offshore Calendar is coordinated centrally before publication.

A proposed calendar of national & local events should be discussed with promoters and clubs before announcing dates.

Regardless of whether the interested parties agree or disagree, it shall remain up to the organisers to sort out their responsibilities.

### **101.03 - DEADLINE DATES**

Organisers may announce any date they wish at any time without penalty.

### **101.04 - CANCELLATION**

Cancellation is solely the responsibility of the organisers.

## **102 - CHAMPIONSHIPS**

World Championships will only be recognised when there are international entrants and a minimum of at least 5 entries per class. In the case of declaration of a world championship without these criteria being met the winners shall not be able to claim UKOPRA World Championship status.

National Championships will be recognised for classes that have a minimum of 5 registered teams and participating at each event

## **103 - RACES RECOGNISED**

UKOPRA recognise the validity the events sanctioned by other bodies including, but not limited to OCRDA, UIM and national offshore authorities outside of the United Kingdom.

## **104 - NON-APPROVED RACES**

Members are not restricted from taking part in any races not sanctioned by UKOPRA however a report of serious infraction at one of those events may result in removal of the participants licence.

## **105 - TITLED EVENTS**

All championship events are allocated with the following minimum criteria:

- Experienced officials and organisation
- Medical rescue
- Experienced staff Permission to hold the event from local authorities
- Events to be run in area with good places for pits and harbour, ample water areas as well as good spectators' areas and services
- Adequate toilets and showers (hot)
- Accommodation and food places must also be located nearby.

The authority may refuse to sanction any meeting which fails to support racing or provide adequate facilities and safety assets.

## **70 - RACING LICENCES**

UKOPRA licences are issued for the sole purpose of competition within the recognised Class 3 and Marathon classes listed.

All craft competing in the above classes will carry a minimum of two fully licenced individuals either of which is fully capable of taking control of the craft at any stage of the race.

Licence applicants that have no previous racing experience are to approach UKOPRA and they will be provided with assessment by experienced racers prior to issuance of any licence.

### **70.01 - COMPULSORY LICENCE**

No one can take part in a race, establish or improve on a record, or test drive at any titled event as a driver or crew in International class boats, without being in possession of a licence from UKOPRA.

Licence holders from an alternative governing body outside of the UK will be granted a UKOPRA extension to their licence for the sole purpose of competing within UKOPRA sanctioned events.

### 70.01.01

In order to obtain a licence, the following disclaimer must be signed by the licence holder:

“The undersigned ... licensed driver (licence n° ...), participating in this sanctioned event, yields the media rights of any coverage by the promoter taken during the event, for further use press, radio or television. The right however remains the property of the undersigned.

In addition, I herewith agree and acknowledge the following:

The organisers preferred authority is the governing rule making body of racing events which are the sole responsibly of the organisers and may be deemed to have delegated authority for their contracted promoters or other organisers.

The sport and technical rules are intended to minimise risks, but they cannot provide the highest possible safety standards at all times. Residual risks will inevitably remain and any person entering is aware that by signing on or entering they waive all rights to litigate for failure or other activity except when caused by actions directed by pre-mediated malice.

Enforcement of the rules by race officials and in particular but not limited to the technical scrutineering does not guarantee the safety of racing or the safety of the scrutineered boat. Scrutineering is not intended as a construction/ condition survey. The racing license issued by UKOPRA does not guarantee that a driver is physically able to race safely nor that he has sufficient experience or education.

Teams and drivers are solely responsible for their own safety including but not limited to their physical and educational ability to race in the relevant class, the safety of their boats and other gear and the safety of their racing activity. This responsibility includes racing with prudence and taking technical measures which are not mandatory in the rules but deemed necessary.

The authority and organisers shall not be liable for any damage, injury or death due to inadequate rules, breach of existing rules by participants or failure to enforce rules by the race officials.”

The signed disclaimer must be signed at all sanctioned events. Should a disclaimer be lost or not signed then acceptance of the rules of the authority and organisers shall be assumed to constitute a contract and acceptance of consent for the disclaimer. Acceptance shall begin at the moment an entry is accepted or competitors and officials enter the areas under the organisers control.

## **70.02 - REGISTRATION**

UKOPRA establishes each year a list of people to whom licences have been delivered.

Any person wishing to obtain a licence must apply to the UKOPRA directly.

The list must include: the name and surname, the address, the class(es) and the Appendix Anti-doping consent form duly signed per acceptance.

These lists may be sent to others National Authorities upon request.

The licence is granted by The National Authority and is marked “as decided by The National Authority.

The digital version of the currently valid Offshore rulebook will only be made available by The National Authority.

The Authority has the right to deliver licences to:

- its nationals;
- nationals of another country.

You are allowed more than one driver's licence.

A National Authority may refuse to give a licence without having to state the reason. Notification of this is sent to all affiliated clubs.

### **70.03 - VALIDITY OF LICENCES**

Licences are valid for 1 year, unless local circumstances require other dates, but validity should always be shown on the licence itself.

An International licence is valid in any country affiliated to the authority and entitles the holder to enter or drive his boat in all competitions organised in any affiliated country, provided he complies with what is stated in the general racing rules.

### **70.04 - COST OF LICENCE**

The cost of the yearly licence is to be fixed by The National Authority.

### **70.05 - EXAMINATION OF LICENCES**

At any meeting, the holder must produce his licence at the request of any official of the meeting.

### **70.06 - CANCELLATION OF LICENCE**

Anybody who takes part in an unauthorised meeting will not automatically lose his licence.

Cancellation shall be the decision of the National Authority. Cancellation shall disqualify the holder from racing for the period designated.

### **70.07 - PSEUDONYM**

The use of a pseudonym must be requested from The National Authority and The National Authority marks the pseudonym on the licence.

A licence holder, so long as he is listed under a pseudonym, may take part in any sports event under any other name provided the licence holder provides the authority and organizer of the event with the real details of identification in advance.

Changing a pseudonym is subject to the same formalities as the adoption of it. Anyone who has chosen a pseudonym may not return to his actual name until The National Authority has agreed and has delivered a new licence.



## **70.08 - ELECTION OF DRIVERS REPRESENTATIVE**

There will be a Driver's Representative from each racing class.

The election of each representative must be agreed by a majority of at least 75% of the drivers present. Candidates must be real drivers. To be eligible, they must have obtained the following minimum experience:

Driver's Representatives for events are elected for the period of the event only. Licence holders are automatically members of the authority and should make other representations at the meetings and AGM.

## **200.01 - RACE ORGANISATION**

The following bodies have drawn up his rule book and retain the right to organise any power boating event: BPRC, IPRC, 2017MOTO with UKOPRA.

Affiliation to The National Authority implies acceptance of the National Rules.

A club, a club member, or a driver who is suspended, is not allowed to take part in any UKOPRA sanctioned power boating event as long as the suspension lasts.

A member or a driver who takes part in any power boat race in a foreign country, undertakes ipso facto to abide by the rules of The National Authority of that country.

All clubs, race organisers, officials and drivers are obliged to know the general rules as well as the rules related to races or series (local, National or International).

The current version of the Offshore rulebook will be made available for consultation at every race in digital version only.

All offshore races organised by National Authorities or their affiliated clubs must be organised in accordance with the UKOPRA Offshore rules.

The general racing rules are applicable to all series.

Any member or driver who infringes any of the rules of the rulebook or any rules of the responsible organiser is liable to be penalised.

All organising bodies, members, drivers or owners who take part in a race pledge themselves:

- to know current Offshore rules and SOLAS regulations pertaining to their area, if any, and also any particular event rules imposed by the event organiser or National Authority.
- to bind themselves to submit without restriction to any consequence following the application of these rules.

## **200.02 - INTERPRETATION OF THE RULES**

In all cases not foreseen by the International rules or seemingly inaccurately defined, The Race Committee, or Race Jury or the Appeal Board, is to judge bearing in mind the spirit of the disputed rule.

The rules for series and classes are to be considered as being supplementary to the Offshore general rules.

In case of conflicting rules, those concerning a specific case/occasion (i.e. series and classes) are to prevail over the general rules.

Organisers may impose special event rules relating to exceptional local requirements. These special event rules prevail over but must not conflict with the Offshore rules.

English is the working language.

## **200.03 - MONETARY PENALTIES**

At any titled International event, the Race Committee shall be entitled to impose monetary penalties for breaches of the Rules; either the Offshore Rules or special event rules.

The monetary amount shall be scaled according to the severity of the “breach of the rules”.

All monetary fines shall be based on a standard monetary value. For the purpose of these rules the value shall be expressed in Pounds Sterling and converted at the current rate of exchange to the currency of the country where the event is being held.

Examples of penalties:

Crew or team members smoking in pit area £50.00

The Race Committee should take care not to substitute monetary fines for infringements of a technical nature or infringements that would normally result in disqualification or suspension.

The fines must be paid before the start of the race; otherwise the driver is disqualified.

Any fines collected shall be deposited in the Race Organiser's bank account and may eventually be used to defray organisational expenses.

Absence from Drivers meeting or briefing

Complete absence from briefing or drivers meeting entails disqualification.

## 201 - RACE CONTROL

### 201.01 - ORGANISING BODIES

All races are under the control of an Organising Body affiliated to UKOPRA of the country in which the race is run. The Organising Body nominates:

- The organisation which organises the general and administrative aspects of the event.
- A **Race Committee** which organises and supervises the actual racing, determines any special event rules, determines the course, decides whether competitors are eligible or not and is responsible for safety. The Race Committee is composed of the following:
  - The Officer of the Day
  - The Safety Officer
  - The Race Secretary
  - The Officials in charge of the Technical Inspections, Course, Wet/Dry Pits, Starting and Time keeping
  - Any other special official appointed by the Organising Body.
- A **Jury**. Nominees must be authorised by the National Authority.

The Race Committee must inspect and verify all entrants documents required prior to racing, i.e., Measurement, Certificates, Licences, Insurances, etc.

The Jury decides on any protest submitted. The names of the members are to be printed in the official programme.

No officer, official, nor member of the Jury nor member of a national authority is allowed to intervene in the judgement of a problem in which he is involved himself or is an interested party.

The composition of the Race Jury must be announced at drivers briefing and published in a race bulletin.

The decisions of the Race Committee and the Jury must be based upon the rules as far as they may apply, but as no rules can be devised capable of covering every case, the Race Committee and the Jury should discourage all attempts to win by means other than fair racing, superior speed and skill.

The following are the Race Officials:

- The Officer of the Day
- The Safety Officer (deputy to the O.O.D.)
- The Race Secretary
- The Technical Officials
- The Course Officials
- The Starting Officials
- The Timekeepers
- The Pit Officials
- The Medical and Rescue Officials
- The Communication Officials.

### **201.02.1 - THE OFFICER OF THE DAY**

The Officer of the Day must direct the organisation of the races on behalf of the organising Body. He supervises the driver's briefings, maintains order on the course, ascertains that all services work properly, ensures that all correspond to the rules and observe the Racing Rules according to the programme. He analyses the written reports of the Officers and Timekeepers and any other documents that will enable the results to be compiled. He verifies any decision to stop or curtail a race. He is not allowed to sit on the Jury.

The Officer of the Day must be approved by the organisers and national authority. To qualify, he/she must meet the following minimum standards:

- Have had previous experience in controlling at least important races.
- Have a good knowledge of the Offshore rules.
- Have the ability to speak and understand the English language.
- Be nominated by his/her National Authority for "Officer of the Day" approval.

A list of approved persons is held by THE NATIONAL AUTHORITY and reviewed each year. Nominations for O.O.D. approval shall be sent to the UKOPRA by the applicant, accompanied by details of his/her qualifications and previous experience.

National Authorities must keep a list of their approved persons up to date and notify the organisers accordingly of any changes.

National Authorities are strongly encouraged to arrange for their Officers to attend training sessions.

### **201.02.2 - THE SAFETY OFFICER**

The Safety Officer is deputy to the Officer of the Day. He / She coordinates (in harmony with the O.O.D. and the medical and rescue officials) the safety services on shore, on the water and in the air.

At the scene of an accident a qualified doctor can take command of emergency services and actions.

### **201.02.3 - THE RACE SECRETARY**

The Race Secretary is responsible for the general organisation of the event.

He / She must insure that all the various officials know their respective duties and that they have the necessary documents and equipment to fulfil their duties.

He / She must collate all documents following the race.

### **201.02.4 - THE TECHNICAL OFFICIALS**

The Technical Officials are to make sure that all drivers have their Measurement Certificates and Homologation Sheets (or production verification) available for inspection and shall verify that the hull and engine(s) conform to the measurement certificate, homologation sheets and the rules.

They check control and safety devices for boats and drivers as provided for in the rules. Scrutineers should be specifically delegated for this duty.

Special attention must be paid to engine and fuel shut-off devices (where applicable), driver protection and safety devices, fire protection equipment and that the racing number is clearly visible.

After the race, they must carry out all the necessary checks.

It is not permitted to disqualify any participant for reasons that were visible during pre-race inspection.

### **201.02.5 - THE COURSE OFFICIALS**

The Course Officials must ensure that the drivers and boats conform to the racing rules.

At the end of the race, they draw up and sign a statement certifying that all participants conduct during the race and at the turn marks was according to the rules. This statement is handed to the Officer of the Day who will apply the International rules in the case of infringements. The Course Officials monitoring turning marks must have a clear un-obstructed view.

### **201.02.6 - THE STARTING OFFICIALS**

The Starting Officials are responsible for the safe and orderly execution of the race start. They must closely observe all boats during the starting procedure to ensure that the rules are adhered to and report any infringements.

One Starting Official must be on the start boat.

### **201.02.7 - THE TIMEKEEPERS**

The Timekeepers are responsible for recording the start time, all lap times and the finish time and finish order of a race. They must be located at a position where they

have clear un-obstructed viewing of the start and finish lines. In some races they may be required to carry out lap timing at turning points other than just the start and finish lines. Timekeepers must work in teams of at least two persons for each timing point. They must use reliable synchronised timing apparatus. They must keep detailed records of the timing of each boat and hand these to the Race Secretary at the completion of the race.

### **201.02.8 - THE PIT OFFICIALS**

The Pit Officials maintain order in the wet and dry pits. They are responsible for the parking of boats, the fueling of boats, the launching and retrieving of boats and all safety procedures within the pit areas including fire safety.

### **201.02.9 - THE MEDICAL AND RESCUE OFFICIALS**

The Medical and Rescue Officials are responsible for all first aid and rescue procedures. The Medical personnel must be fully trained in emergency accident procedures with emphasis on resuscitation, spinal type injury care and trauma treatment. They must have facilities available for emergency treatment and facilities for rapid transportation of seriously injured persons to the nearest hospital.

The Rescue Officials must ensure that adequate rescue boats and rescue helicopters when required and available with trained personnel at strategic points of the race course and that they are equipped for any rescue eventuality.

### **201.02.10 - THE COMMUNICATION OFFICIALS**

The Communication Officials are responsible for maintaining radio or other alternative means of communication between all key race officials with emphasis on communication between rescue crews and race headquarters.

## **201.08 - THE NATIONAL AUTHORITY**

UKOPRA reserve the right to appoint a commissioner to each and any event held under these rules the purpose of which is to report back to the committee on the functioning of the event.

### **2. Qualification of a National Authority Representative**

A UKOPRA Commissioner must have at least a proven experience as a race official on a national level (e.g. O.O.D.), involved in the management of races, or involved in at least race events.

Moreover, he or she

- must lead by example, be integer, promote the positive aspects of the sport and promote fair play
- must have very good knowledge of the rules and guidelines for the specific class he/she is appointed
- must be friendly and approachable, visible and accessible to the competitors and the local organizers (=present all day long each day of the event)
- must avoid conflict of interest must speak, understand and write good English (any other language is an asset)

He/she shall represent the UKOPRA honestly, respectably and with integrity and with comprehension for the local culture and mentality differences.

## **202 - ADVANCE PROGRAMME**

### **202.00 - ADVANCE NOTICE**

To promote events, an advance notice should be published at least 1 month ahead of the event. The advance notice should contain the following information:

- The championship title and class(es).
- The date and place of the event.
- The name, address, phone, fax, e-mail, website (if any) of the organiser.
- Any other information that is available for promoting teams to attend the event.

For Marathon races, the organisers must provide full details of the race entry fees and conditions.



## **202.01 - ADVANCE PROGRAMME**

Advance programmes shall be made available with the following information as a minimum:

The name, the address, the e-mail of the organiser.

The classes eligible.

The venue and the date of the race(s).

The provisional course(s).

Offshore rules may be added or modified.

The closing date and place to which entries must be sent, and whether by post, by e-mail or by fax.

An entry form may be included in the advance programme.

The amount of the entry fee and details of whom payment is to be made to.

All information regarding prizes for each race.

Where and when the racing instructions can be obtained from the organising committee.

Transportation facilities and freight concessions.

Should no publicity be allowed during a race, or there be a restriction on any advertising, this is to be mentioned in the advance programme.

The choice and efficiency of a racing vest is the sole responsibility of the wearer. The wearer is entirely responsible for the choice of his helmet.

Dates and times for registration and scrutineering.

The protest fee.

Details of compulsory insurance for personal and material damage to third parties including other entrants, where required by National Authorities.

The amount of the insurance fee to be paid prior to racing to the organising committee.

The maximum amount for a protest fee in an International event will be decided from time to time.

## **202.03 - MODIFICATION TO ADVANCE PROGRAMME/ RACING INSTRUCTIONS**

No modification is to be made to any particular rule after the advance programme or racing instructions are posted, except if decided by the Race Committee and for cases of force majeure only. Such modification shall be communicated to each and every driver and crew person in writing at an official drivers meeting.

## **203 - ENTRIES**

### **203.01 - RATIFICATION**

The National Authorities must ratify in writing any entry for drivers.

Direct correspondence between the Organising Committee and foreign drivers is permitted.

The Organising Committee will keep blank entry forms at the disposal of the drivers.

An entry is not valid unless it reaches the Organising Committee within the time prescribed in the advance programme and the entry fee is paid.

Entry forms must include the names of all drivers, including ratified test drivers.

### **203.02 - ADMISSION**

No National Authority, or Organising Committee, organising an International race has the right to refuse the entry of a foreign driver duly commissioned by his National Authority, provided said driver is not within a period of expulsion and he conforms to the UKOPRA Offshore rules.

The closing date for entries should be at least 1 day before the event. Owners need only to supply such particulars as are applicable to the class for which their boat is entered.

### **203.03 - ENTRY FEE**

Each Organising Committee fixes the amount of the entry fee for a race.

The National Authorities or organiser have the right to establish a rule whereby higher fees are imposed on late entries. No late entry may be accepted after the closing time of registration for an event.

### **203.04 - MINIMUM AGE**

No person under the age of 18 will be issued with a licence to control the competing craft.

### **203.05 - OWNERSHIP OF BOATS**

The nationality of a boat is the nationality of the owner. The nationality of the driver licence can be taken into consideration in the classification if requested but may be refused.

A boat is allowed to race only under its own name. The owner and the driver may only race under their own name or the pseudonym written on the licence.

### **203.06 - PUBLICITY ON BOATS**

Advertisements may be freely applied to the entire surface of the boat, but there must be a free distance of space of no less than 0.1 meter around the race number.

No owner or crew can be compelled to carry any advertising whatsoever on their person, clothes or upon the boat that is being raced except for markings required by the event organisers to identify competitive craft. Refusal to do so may result in refusal to allow to their participation in any race.

Penalties may be applied to any driver or crew member who, in the course of any offshore racing event, has made any statement contrary to the truth or has distorted the facts.

Political publicity or immoral advertisements are not allowed.

Should restricted publicity or no publicity be allowed for an event, this must be announced in the advance programme.

Any National Authority has the right to fix its own rules regarding publicity for its own boats and venues.

### **203.07 - TEAM REPRESENTATIVE**

At any titled event, each boat must designate its representative for use in case of an accident or force majeure.

## **204 - PROGRAMME / RACING INSTRUCTIONS**

### **204.01**

The Organising Committee must clearly state the following points in their programme / racing instructions:

- The venue and the date(s) of the race(s).
- The date and times of driver registration.
- The dates and times for scrutineering.
- The date(s) and time(s) for drivers briefing.
- Exact, complete description of the race course(s), including all turning marks, known hazards and any designated spectator areas.
- Starting area and its marks.
- Finishing area and its marks.
- Starting time of the race(s), time limits for late starters and finishers.
- Time when the control closes for each race (sign off time).
- Signals of race cancellation, postponement, recommencement of a race, curtailment of a race, or stopping a race.
- When helmets and racing vests must be worn.
- The choice and efficiency of a racing vest is the sole responsibility of the wearer.
- The wearer is entirely responsible for the choice of his helmet.
- Special harbour conditions and local laws, if any.
- Full particulars of prizes for each race.
- Time and place of the prize giving.
- Time limit and place for the handing in of protests.
- Composition of the Race Committee and the Jury.

### **204.2 - DRIVERS BRIEFING**

A driver's briefing must be held before the race.

It is mandatory for drivers to attend the drivers meeting(s).

At least one of the crew, or as described in the Race Instructions, shall attend the briefing(s), otherwise the crew shall not be allowed to start.

It is the drivers' responsibility to read the Race Instructions and have full knowledge of its content before attending the drivers meeting(s).

Any infringement to the rules specified in the Race Instructions may incur a penalty or disqualification.

The drivers briefing shall take place at least two hours (except in cases of force majeure, one hour) before the start of the race and shall be recorded on audio or video.

The briefing must cover:

- Name call or sign in
- Important items not mentioned in, or changes from Race Instructions which must be in writing
- Important local conditions and rules
- Special safety routines
- Weather forecast
- Telephone number of Race Secretary

The members of the Jury and the Commissioner(s) should attend.

## **205 - SAFETY RULES**

### **205.01 - NATIONAL AUTHORITY**

The National Authority requires all drivers under its jurisdiction to declare themselves medically fit to participate and where applicable, an immersion training annually.

Medical declaration may raise an issue that requires further examination and the applicant will be required to satisfy all requests by the UKOPRA Medical Officer. Non satisfaction will result in denial of licence.

### **205.02 - MEDICAL EXAMINATION**

The medical Officer who is to be present at any race has the right to submit any driver entered to a physical or psychiatric examination, or an alcohol and/or drug test. These examinations/tests may be performed at any time during the event.

The result of such examination/test is immediately communicated to the Race Committee, who basing themselves on the report of the medical officer, may suspend any driver from the event.

### **205.02.01 - CONSUMPTION OF ALCOHOL**

Competitors shall not be under the influence of alcohol while competing or practicing.

A competitor shall, if requested by an authorised officer, submit to an alcohol test. Written notification of selection for testing will be handed to a competitor by an authorised official. Refusal or failure to do so may be taken as if a positive test had been ordered and dealt with accordingly.

Testing when carried out shall be using an Alcoholmeter.

Any competitor found to have an alcohol level more than 0% of that permitted by the Road Traffic Act governing domestic motoring regulations of the country which is organising the race shall be immediately suspended and disqualified from the whole event.

If no limit is specified by the organisers of the race, then the level should be zero

### **205.02.02 - ANTI-DOPING REGULATIONS**

Anti-doping regulations based upon the world anti-doping code. As per separate publication.

### **205.03 - MEDICAL EVALUATION SHEET**

Medical evaluation will be undertaken by UKOPRA on a self-declaration basis. Completion of the self-declaration is not a guarantee of licence issue and will be subject to further evaluation if an issue is highlighted.

UKOPRA licence holders may be required to submit themselves for examination at the request of an organiser to the event medical officer. Subject to that examination a licence may be suspended with no right of appeal.

### **205.05 - INJURED DRIVERS**

If a driver has an accident needing medical care, he must immediately inform his National Authority.

It is the responsibility of his N.A. to have the driver examined and verified recovered before the driver is allowed to compete in an International race.

If his N.A. does not manage to verify this (lack of time etc.), the N.A. must notify the organiser and the driver must be examined by the Medical Officer of that race.

## **205.08 - SAFETY BOATS**

Safety Boats in sufficient number must be standing by during official practice and for the duration of all races. Each boat must carry:

- A minimum of two persons who can carry out rescue procedure
- Necessary signal flags
- A large capacity fire extinguisher suitable for fuel fires
- Ropes suitable for towing and a boat hook.

All safety boats must have radio communication with race headquarters.

It must be clear before the race, who has the medical responsibility during the race and a plan in place for medical care and recovery.

## **205.09 - INSURANCE**

All events must be properly insured with the policy to be produced before the start.

Competitor risks to third parties outside of the event should be covered subject to the competitor following the guidelines to responsibility noted elsewhere.

Competitors should be advised that it is advisable to carry their own insurance to cover any risks resulting from non-adherence to the event organisers instruction briefing and this rule book.

## **205.10 - TECHNICAL PROTECTION**

It is forbidden to start a motor with the propeller rotating in the air.

After launching a boat, it is forbidden to start the motor(s) or the engine(s) with the boat elevated and the propeller(s) rotating.

## **205.11 - PROHIBITED SMOKING**

All smoking of any type is prohibited in pit areas. Adequate signs will be posted to this effect and the rule will be rigorously applied by the pit officials.

## **205.14 - FUELLING**

Any team who dispenses fuel, either in or out of the boat, other than in the designated fueling area, or by prior approval of the Pit Officials, will be subject to a penalty as decided by the Race Committee.

### **205.15 - ONBOARD VIDEO EQUIPMENT**

If used, onboard video equipment internal to the cockpit must be securely mounted.

Handheld video use is not permitted while underway, racing, or testing during the event. Under no circumstances may cameras be mounted to helmets

### **206 - ASSISTANCE IN ACCIDENTS**

It is mandatory for competing boats to stop and the drivers to provide assistance at the scene of an accident involving other competing boats, if no other suitably qualified rescue assistance is present.

Placing and championship points shall be awarded to any boat/driver that stops at the accident scene and provides assistance. Such placing shall be related to the last confirmed position it held prior to the time of stopping at the accident scene after deleting all non-finishers from the placing list.

However, except in extreme circumstances as decided by the Race Committee, the boat/driver that gives assistance must, when relieved of their duties by suitable rescue personnel, continue in the race until the completion.

Providing such competitor does finish at the completion of the race, at racing speed, the 70 % rule shall not be applied to that boat/driver.

Should such boat/driver improve its position, it will keep its better place.

Any prize money must be paid for the position awarded.

Penalties imposed by the ODD for reason of failing to stop in an accident or dangerous driving must be imposed as soon as possible on receipt of the evidence of the offence. In a multi-heat championship this must be before the next heat.

The deadlines in 0 .02 do not apply to this rule.



## **206.02 - HAND SIGNALS**

The following hand signals are to be used by drivers and / or crew members at an accident scene to advise rescue personnel of their requirements.

### **HANDS CLASPED OVER HEAD**

OK - No help needed at this time

### **THUMBS - UP**

OK

### **BOTH HANDS WAVED OVER HEAD**

Need immediate medical assistance

### **ONE HAND WAVED OVER HEAD**

Need immediate tow or fire extinguishing assistance

### **NO SIGNAL**

Immediate response by nearest personnel

### **ONE HAND RAISED WITH CLENCHED FIST WHILE ON PLANE**

Caution - Debris or accident ahead!

Slowing or avoidance maneuver to follow.

All drivers, before being issued with their annual licence, must be tested on these signals.

## **207 - DISCLAIMER**

By participating in UKOPRA governed races any participant (driver, team member, spectator, race official or other involved person) acknowledges the following:

UKOPRA is the governing rule making body of UKOPRA racing events which are responsibly organised by clubs, national authorities of THE NATIONAL AUTHORITY, contracted promoters or other organisers.

These organisers do not act on behalf of the UKOPRA and UKOPRA is not responsible for any act or omission of such organiser and shall not be liable to the participant accordingly.

The UKOPRA sport and technical rules are intended to minimise risks but they cannot provide the highest possible safety standards at all times. Residual risks might remain.

Enforcement of the rules by UKOPRA or other race officials and in particular but not limited to the technical scrutineering does not guarantee the safety of racing or the safety of the scrutineered boat.

Scrutineering is not intended as a construction/ condition survey.

The racing licence issued by UKOPRA or a super licence issued by the UKOPRA does not guarantee that a driver is physically able to race safely nor that he has sufficient experience or education.

Teams and drivers are solely responsible for their own safety including but not limited to their physical and educational ability to race in the relevant class, the safety of their boats and other gear and the safety of their racing activity. This responsibility includes racing with prudence and taking technical measures which are not mandatory in the rules but deemed necessary.

UKOPRA shall not be liable for any damage, injury or death due to inadequate rules, breach of existing rules by participants or failure to enforce rules by the race officials.

When race boats are being craned in and out of the water prior to, during or after a race meeting, the boat shall be craned without any person on board. The arrangements for craning shall be so organised that on no account will the craned boats pass over the head of any person.

## **208 - CRANE AREA**

This area must be cordoned off and marshalled.

### **300 - RACING RULES**

Offshore races are held in open waters that are accessible by the public.

Accordingly, all competitors are held responsible as they would be under pleasure regulations.

The presence of course marshals is not to be taken as a presumption that the course is clear.

Organisers should make every effort to sanitise a start chute before leading boats into a start run and competitors are minded that a red flag may be shown at any time to abort a start.

### **301 - TIMING**

Timing is carried out by officially appointed timekeepers using certified timing devices graduated to one tenth of a second. The devices must have a split-second system one part of which must be capable of being stopped and restarted.

### **303 - MARKS OF THE COURSE**

#### **303.01 - DEFINITIONS**

The Marks of the Course are any objects (boat, buoys...) that are specifically designated as such in the racing instructions. The objects that are not designated specifically as such are to be considered as obstacles.

An Official is posted near the turning marks. He must observe the turning of the marks and report to the Race Committee.

Should any mark be removed from its proper position either by accident or otherwise, the Race Committee shall, if possible, have it replaced. Should it be impossible to replace the mark in time for the boats to round it, the Race Committee shall decide whether the race is to be restarted or not.

The distance between the starting line and the first turning buoy must be at least one nautical mile.

#### **303.02 - SPECTATOR AREAS**

Race Organisers should designate spectator areas.

If a race boat finds itself in a designated spectator area, it must come off the plane immediately and proceed "off plane" with caution until back on the race course to avoid disqualification.

## **305 - PROCEDURE FOR OFFSHORE STARTS**

### **305.01**

A safe area shall be established for milling. In this area, boats must be off plane (bow down, no wake) and must turn in counter clockwise direction.

### **305.02**

Distinct areas in which each starting group can mill (safe area) will be established. See diagram for area and location.

### **305.03**

A control line on the race side of the milling area which is effectively where the start boat turns to pick up each class to start will be established.

### **305.04**

The referee - wearing orange gloves and an orange vest (optional) over his life jacket on the start boat may use hand signals to control the fleet prior to the start.

### **305.05**

The use of a starter boat is mandatory for all classes.

There must be a safe interval between each class start with the start boat headed towards the start line to one side or the other of the chute.

### **305.06**

The minimum width of starting chute is 20M with a minimum length of 200M. The chute must be straight.

Drivers must keep their lane in the start chute, up to a point presented in Race Instructions.

From there on only "The rules of the road", are valid.

Breaking this rule will result in one or more of the following penalties: warning, time penalty, disqualification, yellow cards, depending on the severity.

### **305.07**

A single white smoking or rocket flare will be fired 2 minutes before the beginning of the starting run-up for the first start class, unless otherwise stated in the race instructions.

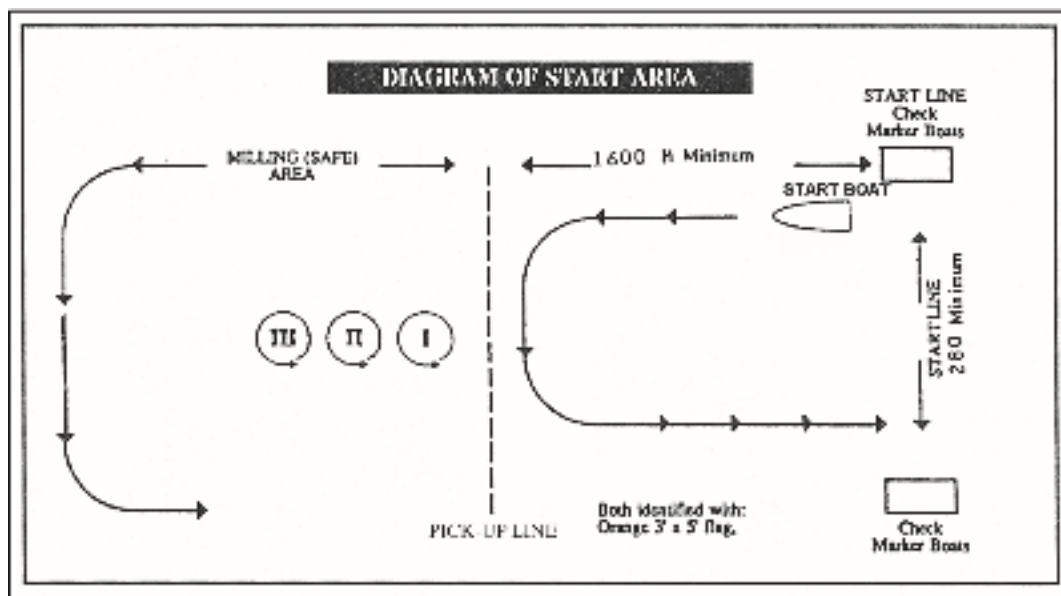
**305.08**

Two minutes following the firing of the start signal flare, the start boat shall raise and continue to display a yellow flag whilst running across the “pick-up” line.

When the start boat reaches the opposite side of the starting chute it shall turn and begin it's run towards the start line.

All race boats may then begin their run towards the start line, following the start boat, at the 30M minimum safety distance, until the raising of the green flag.

The minimum dimensions recommended for the yellow flag are 1.5 m x 1.0 m.

**305.09**

During the start procedure the race boats must run at the safety distance of at least 30M behind the start boat and must observe a safe spreading distance between boats.

**305.10**

The start boat should approach the start line at a speed which allows boats which have difficulty in getting on plane to catch up.

### **305.11**

When the starter is satisfied that all boats are in a satisfactory line, at a satisfactory speed, he raises the green flag and lowers the yellow flag simultaneously. The raising of the green flag determines the actual start.

The minimum dimensions recommended for the green flag are 1.5 m x 1.0 m

### **305.7**

The race only starts when the green flag is raised. However, for time keepers, the official start is when the leading boat crosses the actual start line. The actual start line shall be announced in the racing instructions.

### **305.13**

If at any time during the start procedure after the white flare has been fired, a red flag is raised, the start has been aborted and all boats must return off plane to the milling area for restart or follow specific instructions of the start boat.

One, or two start line/check marker boats may be used in conjunction with other fixed or laid marks help define the actual start line.

### **305.14**

A disabled boat or one which cannot get on plane with its class, must not interfere with the start of the next class in any manner. The craft must go to one side of the chute and wait for a clear lane to commence their start. Should a boat fail to start after its respective class has started, it may start, in a normal fashion within a half hour (30 minutes) of its respective class time of start.

### **305.15**

It is recommended that the start and/or pace boat personnel be limited to seven persons.

### **305.16**

Penalties for infraction of the starting procedure are:

- a) Failing to respect a safety distance 30M - 3 minutes
- b) Turning wrong direction – 3 minutes
- c) Interference with starting procedure - disqualification;
- d) Excess speed in the milling area, or faster than speeds determined in race instructions - 5 minutes penalty. If the driver has caused danger to other boats, he may be disqualified;
- e) Should the bow of any boat be in front of the transom of the start boat when the green flag is raised - 5 minute penalty.

An outer-limit pace boat on the other end of the penalty line may be used to help define the actual penalty line from transom to transom.

### **305.17**

Safety takes precedence over racing. Pilots should do nothing to endanger crew, spectators or other participants.

### **305.18**

If it is desirable to start two or more starting groups together, this may be done provided it is fully covered in the racing instructions (and/or drivers briefing).

### **305.19**

It is recommended that the same start boat starts each starting group and in which case the start boat, having started the first group should return on the diagonal to its original position at the opposite end on the control line and immediately repeat the starting procedure outlined in 305.08 for the next starting group until all the groups have been started. Alternatively, if separate start boats are used for secondary starting groups they should utilise the procedure described in Rule 305 at time intervals fully described in racing instructions.

### **305.20**

Should sea conditions in the milling area be such that boats ship water when turning while off plane, procedure will be modified as follows:

The start boat will lead race boats to the milling area at which point it will raise yellow flag slowly to allow race boats to get on the plane in safety before accelerating towards the start line. Race boats will follow the start boat at the thirty metres minimum safety distance, on the side and in the manner described in the racing instructions or at race briefing, until the raising of the green flag.

### **307 - BAD WEATHER COURSE**

Should bad weather compel the organisers to have any race run on a shortened course and/or an alternative start time, points are granted provided:

That a minimum of hour notification, prior to the start time, of the amendment to the course or the start time is given to each driver in a Race Bulletin. A register must be signed by every driver confirming receipt of the Race Bulletin.

Should the race be stopped for any reason of 'force majeure', the boats will be classified according to 2.0 providing 70% of the minimum race distance has been completed. If this distance has not been completed, no points will be awarded.

### **309 - RACE CANCELLED**

The Race Committee has the power to cancel any race should unfavourable weather or other serious circumstances render such action necessary. A red flag raised indicates that such race has been cancelled.

### **310 - RACE POSTPONED**

#### **310.01**

The Race Committee has the right to postpone a race, in which case the red flag must be raised from the start boat and/or a patrol boat. Race starts thus postponed may re-commence the starting procedure with the raising of the yellow flag as described.

The Race Committee may postpone the start, even after the first signals have been given, but only in cases of force majeure or if an error in the starting signals has been made.



### **310.02**

In both cases, signaling is to be recommenced. When the need for postponement has passed, the Race Committee signals the start according to the instructions contained in the programme.

New entries are not to be accepted for postponed races. Only drivers who duly entered and were present when the race was postponed are admitted.

### **311 - RACE SHORTENED**

A shortened race is a race over a course which has been shortened by the Race Committee before the start.

The decision with instructions about the shortened course must be handed in writing to the drivers and the timekeepers.

### **37 - RACE STOPPED**

The organisers may stop the race at **any** checkpoint of the route before the official finish, and/or if the race has been run more than 1 nautical miles, and/or the drivers have been racing for at least an hour. The race will be approved as a Championship race. The curtailment decision must be ratified by the Race Committee immediately after the race.

A red flag raised indicates the race has been stopped.

A stopped race is a race which has been interrupted by the Race Committee after the start.

Stopping the race is decided by the Race Committee for reason of force majeure of which it is the sole judge.

The Race Committee classes the boats according to their position at the time or place when and where the race was stopped.

### **37.01 - RACE CURTAILED**

A curtailed race is a race shortened by the Race Committee after the start.

The signal to be used to announce it, is the waving of the chequered flag at the finish line.

The O.O.D. or other responsible official can implement a curtailment decision at any time during the event, even in a multi-class race, provided the curtailment signal is broken out on the start/finish line. Once the curtailment signal has been broken out, all boats will stop racing at the end of their current lap.

Raising a retirement flag will not preclude a driver from getting finishing points that would otherwise be awarded if the race is curtailed.

Arriving at a finishing sequence - The first boat in each class, or subdivision, to be stopped by the curtailment signal becomes the "key" boat in that class. Then -

I) All boats that subsequently take the curtailment signal and have then completed the same number of laps as the "key" boat are listed, in their finishing sequence behind the "key" boat.

II) All boats that subsequently take the curtailment signal and have completed one less lap than their "key" boat are listed, in their finishing sequence, behind all the boats listed under (I) above.

III) Boats that break down on their last lap shall be placed in the finishing order, in the sequence which they last crossed the start/finish line and in accordance with the number of laps which they actually completed.

IV) All boats that subsequently take the curtailment signal and have completed one more lap than their "key" boat are listed, in their final finishing sequence, ahead of all boats listed under (I) and (II) above, and probably ahead of all boats listed under (III).

V) Boats that break down on their last lap having completed less laps than any other finisher (under the above rules) shall be regarded as non-finishers.

### **313 - RULES OF THE ROAD**

#### **313.01**

The International regulations for the prevention of collisions at sea, shall apply at all times.

**313.02**

Where by any of these rules one of the two boats is to give way, the other must keep her course and speed.

**313.03**

Every boat, which is directed by these rules to give way to another boat must, if the circumstance of the case admit, avoid crossing ahead of the other.

**313.04**

Every boat which is directed by these rules to give way to another boat must on approaching her, if necessary, slacken her speed, or stop or reverse.

**313.05**

Any boat overtaking any other must give way to the overtaken boat.

**313.06**

In obeying these rules, due regard must be made to all dangers of navigation and collision, and to any special circumstances which may render a departure from the above rules necessary in order to avoid immediate danger.

### **313.07 - OVERLAP**

An overlap may only be established when two boats are on the same course or approximately on the same course and the overtaking boat has no longer a free choice on which side he shall pass.

The inside boat may gain the right of way by establishing an overlap even after the outside boat has altered her helm for the purpose of rounding the mark.

Once an overlap is established during the approach to or the rounding of a mark it cannot be broken even if the inside boat is clear astern.

An overlap exists when one boat is not clear astern of another.

A boat is clear astern of another when her hull is abaft a line projected abeam from the aftermost part of the other.

### **313.08 - PASSING OBSTACLES**

Should an overlap exist between two or more boats when they are about to pass an obstacle, then the outside boats shall give the inside boats room to pass clear of the obstacle.

### **313.09 - PASSING MARKS**

Should an overlap exist between two or more boats when they are about to pass a mark on the required side, then the outside boats shall give the inside boats room to pass clear of the mark, leaving it on the required side.

### **313.10 - ALTERING COURSE**

When one of the boats is obliged to keep clear to avoid risk of fouling, the other shall not alter her course. A boat must not alter her course so as to hinder another in passing to starboard.

### **313.11**

When an overlap exists but neither boat is an overtaking boat, the one which has the other on her starboard side must give way.

## **314 - ROUNDING AND FOULING MARKS**

### **314.01**

Rounding marker buoys must be in the direction and on the side stipulated in the race instructions.

### **314.02**

A driver who is forced on to a mark by another driver may lodge a protest against that driver.

## **315 - FOULING COMPETING BOATS**

If a boat, in consequence of her neglect of any of these rules fouls another boat, or compels others to foul each other, she is disqualified.

## **316 - RUNNING AGROUND**

Any boat, running aground, fouling a buoy, vessel or other obstruction, may use her own anchors, warps, spars or other gear to clear herself.

## **317 - ANCHORING DURING A RACE**

A boat may anchor during a race but must weigh and recover her anchor again, not slip it.

## **318 - MEANS OF PROPULSION**

All methods of propulsion other than motor, and paddle are prohibited.

The action of the wind and current will not entail the disqualification of the competitor.

## **319 - OUTSIDE ASSISTANCE**

### **319.01**

Unless specified in racing instructions, no outside assistance (including refueling) is allowed during a race.

For all classes it is permitted to use electronic steering compasses and/or electronic position fixing equipment. It is expressly forbidden to interface any of this equipment to the steering system.

GYROS : All movable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control loop).

The Race Committee has the right to decide whether there was outside assistance or not.

No changes to the boats systems may be made from outside races (i.e. no radio command links to adjust onboard parameters to engine management systems, either air/fuel ECUs or ignition ECUs).

## **320 - FINISHING A RACE**

### **320.01 - THE FINISH**

The finish of a race is signaled to the driver of the winning boat by waving a black and white flag at the finish line. Where two or more classes are running together, a different flag may be used for each class.

The description of the flag used for signaling the finish of each class must be clearly specified in the Racing Instructions.

When the leading boat completes the course and crosses the finishing line, the chequered flag will be waved. It designates the “winner” of the race and the start of the finish procedure of the race.

Any boat crossing the finishing line after the “winning boat” will be classified considering its time and the number of laps completed, provided that it has covered the minimum number of laps announced at the race briefing.

When calculating the number of laps completed by a boat, its last lap completed (after the arrival of the winning boat) must not be slower than 50% of its fastest average lap speed.

The race may be curtailed or stopped at the finish line at any time by waving the chequered flag. This designates the end of the race. To be designated a “finisher”, a minimum of 70% of the laps completed by the winning boat must be completed, rounded up or down to the nearest full lap unless otherwise stated in the race instructions.

### **320.02 - STOPPING THE RACE**

In case of force majeure or accident, the Race Committee may stop the whole race, waving a red flag. This signal will be given from the turn mark control boats. Additionally this signal may be given by other official safety boats.

### **320.03**

A driver who finishes a race or heat must withdraw from the course without hindering the boats that are still in the race. This constitutes an exception to rule “Right of Way”.

A driver who has finished that race and cuts across the course may be disqualified.

A vessel is timed as completing a race when her bow crosses the finishing line. After finishing the race, she must continue to observe any special regulations prescribed by the Race Committee as to keeping clear of the finishing line and the course.

**320.04**

The Race Committee has the right to stipulate the maximum duration of each race.

**321 - SCORING SYSTEM**

**321.01**

Unless otherwise specified, the following scoring system is to be adopted:

1 – 400 points

2 – 360 points

3 – 225 points

4 – 169 points

5 – 77 points

6 – 95 points

7 – 71 points

8 – 53 points

9 – 40 points

10 – 30 points

11 – 22 points

7 – 17 points

13 – 13 points

14 – 9 points

15 - 7 points

16 – 5 points

17 – 4 points

18 – 3 points

19 – 2 points

20 – 1 points



### **321.02**

National class boats may race at International meetings.

### **321.03**

If boats of different series or classes start together every boat must be classified within its own class.

## **323 - POSTING OF THE RESULTS**

The results of each race must be posted, as soon as possible after the race on the notice board at race administration office (specified during the driver's meeting/or in the Race Instructions).

The O.O.D. and Timing officer must sign together the provisional and official results and state the time of posting on the sheets.

### **A. PROVISIONAL RESULTS**

The first posted results, with "Provisional Results" mentioned on the sheet, remain provisional for hour, unless otherwise stated on the results sheet for purpose of post-scrutineering or other purpose.

Competitors can only lodge protest against results or against other competitors as long as results are provisional (protest time).

The deadline of an hour is not valid for penalties imposed by the O.O.D. for the reason of post-race scrutineering, and under the condition that this is clearly stated on the provisional results sheet. The results will in this case remain provisional until hour after the finalization of the post-race scrutineering.

Once post-scrutineering is finalised, the technical scrutineers in charge will report this to the O.O.D, who has to record the finish time of post-race scrutineering. hour after the end of post scrutineering, if no protests lodged, results will be made official.

All competitors and jury members must remain available on the event site during the hour after posting of provisional results or hour after finalisation of post-scrutineering if results remain provisional till the end of post- scrutineering. This is the post-race protest period for each race.

Protests (cfr. 0 Protest procedures)

During the protest time competitors will able to protest.

A protest against new results which are not caused by a jury decision can be lodged within one hour of these new results being posted, but only concerning the decision

(e.g. post-scrutineering, etc.) which led to the changing of the results.

If protests have been lodged, these protests will be reviewed by the Jury and hearing must be organized.

The Jury must decide as soon as possible on the protest presented to it and will make a decision, unless otherwise stated in the jury decision. And then the "Official results" will be published with the signatures of the O.O.D. and the jury or committee.

No protest is permitted on Jury decisions.

If there has been no protest after expiry of the protest time, the O.O.D. will sign and so approve the results as official.

## **B. OFFICIAL RESULTS**

After 1-hour provisional results are posted, or in case subject to post race scrutineering, after 2 hours after the finalization of post-race scrutineering, and if there has been no protest, after this expiry of the protest time, the O.O.D. will sign and so approve the results as official, with "Official Results" mentioned on the sheet.

A copy of the official results, signed by the O.O.D. will be published on the notice board in Race Administration Office. As from that period no more protests can be lodged by competitors.

For some specific classes or cases, results can remain provisional, not only subject to post race scrutineering, but also for other reasons.

These reasons have always to be clearly stated on the provisional results sheets, such as: - "Subject to the results of routine fuel analysis for the following boats...."

- "Subject to further checks to be carried out for boat N° ...."

- "Subject to the result of the appeal lodged by the competitor of boat N°..."

- "Subject to the results of a complete engine inspection or dynotest for boat N° ..." -  
"Subject to the results of the anti-doping tests for the following crews..."

## **324 - INSPECTION OF HULLS AND ENGINES**

### **324.01**

Boats, and engines taking part in a race which are subject to checking, must not leave the boat park until one hour after the positing of the results.

### **324.02**

The Technical Officers may postpone the inspection and stipulate the time limit and place where the engines must be available.

### **324.04**

In the meantime, the motor must remain sealed to prevent any alterations being made. Refusing to produce the motor or the hull entails disqualification.

### **324.03**

It is compulsory for a driver to make arrangements for tools, special tools and mechanics so that his motor can be disassembled if the Technical Officers at a post-race inspection want to check any part of the motor. Both the driver and/or the mechanics shall work as efficiently as possible so that inspection duration can be kept to a minimum.

Motors and boats will be placed pending inspection in a well-delimited place, which remains under the care of the "boat park " Official. This area is called the "parc fermé "  
.

### **324.05**

After the pre-race technical inspection, the boats must not leave the boat park without permission except for practicing or racing.

**324.07**

**326 - AFTER THE EVENT (REPORTS)**

**324.06**

If any repairs can only be effected outside the boat park, they must be carried out in the presence or with the permission of a Technical Officer.

Officers allowed to attend the inspection:

The Officer of the Day.

The delegate of the interested National Authority.

The measurers named by the organising committee.

The driver and a mechanic if necessary.

The presence of other persons is only allowed if authorised by the Race Committee.

**326.01**

At the end of the event, the Secretary of the Race Committee must send to the National Authority a report including:

Classification and distribution of prizes (minutes of the jury)

Report of the timekeepers

List of drivers

Penalties

List of protests and decisions relating to these protests.

If the results are taken into consideration for the granting of a trophy or a prize, the points must be sent to the UKOPRA by e-mail within hours.

2 days after a competition, the organising club must send the final result to the National Authorities of all the participants if requested.

## **400 - JURISDICTION**

### **401.02**

No claim for damages arising from an infringement of any of THE NATIONAL AUTHORITY or organisers rules or the Advance Notice or any of the Race Instructions or Race Bulletins shall be adjudicated upon by any Race Committee or appeal authority but shall be subject to the jurisdiction of the courts.

The crew of a racing powerboat which acknowledges infringing a rule does not thereby admit liability for damages.

The findings of fact, and the decision of the protest committee shall be relevant only to the purposes of the powerboat racing rules and shall not be referred to in any proceedings for damage without the written consent of all parties to the protest.

## **402 - JUDICIAL BODIES**

### **402.01 - THE JURY**

The Jury hears any protest raised by a driver.

#### **1. Composition**

The Jury must be composed of at least three members, the Chairman included. The Organisers of the event appoint the Jury. These appointments must be made in writing, authorised and signed by an Official.

If there are not enough qualified members to form a Jury of three persons, the Organisers must appoint additional members, but only enough to reach three persons.

The Jury composition will be announced at the first drivers meeting. The Jury must be present at the first drivers meeting.

Race Officials cannot be Jury members.

In an event comprising races of several classes, it is possible to keep any authorized jury member to adjudicate on any matter of the jury, irrespective of the class he/she happens to represent.

## **2. Authority and Duties**

The Jury sits as long as the event lasts, and hears all protests relating to that event. The jury must decide as soon as possible on all protests presented to it during the event.

No member of a Jury who has taken part as a competitor or Official in the race concerning which a judgement is to be given, or knows himself to have directly or indirectly an interest in one of the parties involved, may judge the protest.

### **402.02 - INTERESTED PARTIES**

However, a member of a Jury shall not be disqualified due to membership of the National Authority.

### **402.03 - APPEALS BOARD**

The jury will decide on Appeals arising from events according to the provisions laid out in the rulebook.

In each case, the Executive Committee, or in case of an appeal lodged against an Executive Committee's decision, the organisers will nominate three members, one to be Chairman after consultation of the relevant commission.

## **403 - PROTEST PROCEDURES**

### **403.01 - VALIDITY OF PROTESTS**

Only a driver may protest against facts pertaining only to the races in which he takes part.

Joint protests signed by several drivers will not be considered.

All protests must be in writing - type written or hand written in printed characters in English.

It must state the reason for the protest and be accompanied by any relevant documents that shall provide evidence within the given time and any protest fee as stipulated by the Race Organiser.

All protests must be signed by the protester.

The protest must be handed to the Race Secretary who must, in the presence of the protester, record the time of receipt.

Any costs involved in verification are to be borne by the losing party. A written protest can be withdrawn by the protester; he then loses the protest fee.

#### **403.02 - PROTEST FEE**

The amount of the Protest fee is £200 unless otherwise noted and must be decided by race organiser and it must be published in the advance programme. The maximum amount for a Protest fee will be decided by the will be £1000 or an equivalent sum in the local currency.

#### **403.03 - RIGHT OF PROTEST**

A protest can be lodged against the posted provisional results or against any decision made by the Race Committee or against one or several competitors.

In the event of a competitor protesting against a penalty imposed on him by the O.O.D. or the THE NATIONAL AUTHORITY and another driver protesting simultaneously against this competitor due to the same incident, the Jury will have only one meeting with both competitors being party of this protest procedure.

A protest can only be lodged on a subject which directly concerns the protester himself. A protest which complies with these rules cannot be refused by the Jury.

No protest is permitted on Jury decisions, they may only be contested by appeal.

#### **403.04 - TIME OF LODGING A PROTEST/ PROTEST PERIOD**

A protest regarding the qualification of a boat, a motor, or a driver must be made before the first drivers briefing. Starting in a race is considered as acceptance of the conditions and the eligibility of the other competitors. The only exception to the above is when the protester can prove the facts were not given to him before the drivers briefing, then a protest can be lodged up to one hour after the posting of the results.

Any other protest must be lodged within one hour of the first results being posted with the following exceptions:

A protest of a driver (driver "A") against another driver (driver "B") can be lodged within 30 minutes after the posting of the protest form. If driver "B" has been penalised and himself protested against this penalty. Without lodging such a protest driver "A" shall not be entitled to appeal against the jury decision.

A protest against any other decision, posted after the posting of the first results can be lodged within one hour after the posting of this decision.

#### **403.05 - JUDGEMENT**

Decisions by the Jury shall be reached by simple majority of votes. In the case of equal votes, the Chairman's vote will be the casting vote.

In the event of a protest, penalties may be agreed, rejected or changed by the Jury. If a protest is upheld by the Jury, the fee must be returned to the protester. Any judgement pronounced by the Jury stays in effect until altered by the Appeal Board.

#### **403.06 - NOTIFICATION OF DECISION**

All decisions by any Jury, the minutes of the meeting and the justification of the decision must be notified to the concerned parties in writing, including their right of appeal.

#### **403.07 - AVAILABILITY OF DOCUMENTS**

All documents relevant to any decision by the Jury must be filed with the organisers and held available until the time limit for an appeal has expired.

### **404 - HEARING OF THE PROTEST**

#### **404.01 - RIGHT TO A HEARING**

Any person being party to a protest, or being charged for an offence against the rules shall have the right to be heard before the Jury in order to defend himself.

#### **404.02 - WITNESSES AND EVIDENCE**

It is the responsibility of the parties involved in a protest to ensure that witnesses appearing on their behalf together with any other evidence are present and the Chairman of the Race Jury at his/her sole discretion may take their availability into account when determining the time of the hearing.

Any costs incurred by the appearance of witnesses shall be borne by the respective parties unless decided otherwise by the Jury.

#### **404.03 - THE HEARING PROCEDURE**

The following procedure must be followed in all hearings unless otherwise stated elsewhere in these rules:

- A copy of the protest must be posted in the same way as the results. The time of posting must be noted on this copy.
- Together with a copy of the protest, the Jury must give written notice to all parties of where and when the hearing will take place. Reasonable time shall be allowed to the parties for the preparation to the hearing.



- If any party duly notified, fails to appear without giving an acceptable reason, judgement can be rendered by default i.e. the missing party shall lose the protest.
- Written minutes must be taken.
- All parties to the case are entitled to be present and to hear and question all evidence at the hearing up to the time the Jury makes its decision.
- The protest shall be read out to the parties.
- A party to the hearing who believes that a member of the Jury is an “interested party” or otherwise not suitable to decide upon the protest shall object at the beginning of the hearing and before he/her states his/her own case. Failing to do so will result in acceptance of the relevant jury member unless the party can prove that the circumstances and facts resulting in the ineligibility of the jury member came to his/her knowledge only after this moment. In this case the party has to object immediately after having obtained the relevant information.
- Then the Jury shall decide, whether all formal requirements of the protest are fulfilled and possible additional rules of the relevant class. Failures shall lead to the protest being void. The parties shall be given the opportunity to give statements if failures are found and before a final decision on the formal legality of the protest is taken.
- The parties shall be invited to state their cases. The parties may call witnesses. Each witness, after having given his account of the case, may be questioned by all parties and by the members of the Jury. Any other evidence available may be presented.
- Any member of the Jury who is familiar with the case may give his evidence
- The parties shall be entitled to question each other and any member of the Jury who may have given evidence.
- The witnesses shall withdraw and the parties shall be invited to make a final statement of their cases.
- The Jury may recall any party, previous witness or new witness and call on any other evidence to verify the facts.
- The parties must be present during the whole of the recall and must be given the opportunity to question any new evidence after which they may re-make their final statements.
- After all evidence has been assessed the situation with the protest may be discussed. The hearing will then be closed and the Jury shall debate the case and take a decision in a closed meeting and no other person but the jury members have a right to be present.

## **405 - THE APPEALS PROCEDURE**

Appeal to UKOPRA will only be allowed in the case of flagrant disregard or abuse of the rules. In all other cases the jury decision to protest should be regarded as final.

## **406 - PENALTIES**

If during the procedure further costs arise, the Appellant has to deposit the missing amount accordingly within days from notification.

The driver who appeals has to pay for the meeting costs anyway if the appeal is upheld or not. If the costs of the appeal committee is less than £350 then the difference will be paid back to the driver.

The appeal fee of will be refunded if the appeal is upheld. If the original decision is changed by the appeal board, it can decide to refund a percentage of the appeal fee if appropriate.

The Chairman of the Appeal Board can decide that the appeal is not considered valid since the expiry dates were not respected. In this case 50% of the received appeal fee will be reimbursed to the appellant.

The driver may agree to forego an Appeal Committee meeting and for the appeal to be handled through telephone, online networks or e-mail, if possible, in order to minimise the cost for the appeal.

### **406.01 - GENERAL**

Any proven breach of applicable Rules may be penalised.

If the rules do not determine a special penalty for the relevant breach, the penalty to be given has to be proportional to the seriousness of the breach. The proportionality is at the discretion of the decision maker.

The Executive Committee or the relevant National Authority shall only impose penalties, when they deem the relevant infringement to be so serious, that it has to be penalised although the deadlines for penalising or lodging a protest have already expired. After the expiry of the aforementioned deadlines, a penalty for an ordinary breach of the rules shall in general not be imposed.

The Race Jury or the Appeal Board following a protest or an appeal can (only) impose penalties, which can be imposed by the O.O.D.

#### **406.02 - DEADLINES FOR PENALISING**

The first posted results are provisional for one hour.

The National Authority and/or the Executive Committee can only impose penalties within 1 month from the day, the infringement occurred.

These deadlines are not valid for penalties imposed by the O.O.D. for the reason of post-race scrutineering and under the condition that this is stated on the results sheet.

In this case the results will remain provisional until one hour after the finalisation of the post-race scrutineering.

The date and time of finalisation of the post-race scrutineering must be recorded by the technical scrutineer in charge.

If the post-race scrutineering cannot be finalised until the end of the event and the O.O.D. has already left the race site the penalty has to be imposed as soon as possible after the receipt of the scrutineering results by the O.O.D.

#### **406.03 - UNACCEPTABLE BEHAVIOUR**

In order to protect the interests of the sporting community, the following actions may also be penalised by the O.O.D., by the Executive Committee or by the N.A. of the Licence Holder.

- any deliberate act taken to gain unfair advantage.
- any false act made or statement given with the intention of suppressing facts required for the proper conduct of the race.
- any attempt to bribe or the taking of a bribe.
- any abusive or unsportsmanlike behaviour.

#### **406.04 - REPRIMAND**

A reprimand is a notice of disapproval of an unacceptable action. It must be recorded by the Race Secretary. A reprimand must be witnessed. A reprimand automatically constitutes warning that if the offence recurs, a heavier penalty will be given. A Reprimand is valid for 2 Months.

A reprimand can be given by the O.O.D., or The National Authority of the Licence Holder.

#### 406.05 - YELLOW / RED / BLUE CARDS

A yellow or red card may be given by the O.O.D. or THE NATIONAL AUTHORITY.

A yellow or red card will be confirmed on a public notice which will be posted with the results. The recipient must be notified privately before a red card is posted.

The driver has one hour from the posting of the penalty to protest. If the protest is not upheld he can make an appeal.

A yellow/red card received in all disciplines and classes count towards the driver's CV and shall be accumulated.

Yellow/red cards can only be given for bad behaviour on the water including:

- up to two yellow cards at a time for dangerous driving;
- one red card for extreme blatant dangerous driving.

A driver who receives a red card is immediately disqualified from that event. Receiving a third yellow card equals to a red card.

A driver with a red card shall lose the right to take part in any sanctioned activities on the water for two months.

In addition, he is not eligible for the first titled race in any discipline and class subsequent to the receipt of the red card.

All yellow cards will remain valid for the events in which he competes following the date on which the last yellow card was delivered but will expire at the end of the same season as published by the event organisers.

Yellow / **red** cards must be notified to the National Authority of the competitor concerned within three days. National Authorities will publish a record when a driver has a red card and is therefore suspended from competition.

These rules also are to be applied to all people in the paddock:

A blue card can be given for any offensive, un-sportsman, abrasive etc., behavior during any official activity (briefing, race control during event etc.)

A second blue card will immediately remove the offender from that event. Following the subject activity, the Race Jury will meet and determine if further disciplinary action is justified. Only the executive committee and OOD may issue blue cards.

#### **406.06 - DISQUALIFICATION**

Disqualification deletes a competitor from the results of the heat or race where the offence occurred. Disqualification is done by the O.O.D.

#### **406.07 - TEMPORARY SUSPENSION**

Temporary suspension suspends a competitor, a competitor together with crew, a crew member, or an Official from all or part of an event. Temporary suspension may be given for serious indiscipline.

Temporary suspension will be imposed by the O.O.D.

#### **406.08 - PROLONGED SUSPENSION**

Prolonged suspension can be imposed on a competitor, an Official or an organisation for deliberate fraud, repeated or very serious indiscipline or very serious misconduct.

Prolonged suspension can be imposed by the N.A. of the licence holder or organisation, or by the Executive Committee.

#### **406.09 - EXCLUSION**

Exclusion means a permanent loss of all rights to take part in any activities falling under the N.A and its affiliated organisations.

A person or an organisation who has committed a moral or sporting offence of extreme gravity is liable to be excluded.

A sentence of exclusion can be pronounced by the N.A. of the licence holder or event organiser, or by the Executive Committee.

In the case of suspension, prolonged suspension or exclusion, the National Authorities and therefore their affiliated clubs bind themselves to respect the decision and apply it in their own events.

#### **407 - PRIORITY OF THESE RULES**

The course of the N.A has to be exhausted, before the dispute can be brought before civil court.

## **408 - POWERBOAT PROTEST FORM**

### **PART A**

Is for you, the driver, to complete and hand in within the specified time limit.

### **PART B**

#### **DRIVERS PROTEST FORM PART A**

Is for you, the driver, to complete and give to the Protest Committee Chairman at the beginning of the Protest Meeting.

### **PART C**

Is for the Protest Committee to complete.

## **SECTION 1**

Name of Event:

Date of Race:

Class:

## **SECTION 2**

Your Name (Protestor):

Your Boat number:

Boat Name:

## **SECTION 3**

I (the Protestor) am protesting against:

- To seek redress from the Race Organising Committee for actions or omissions
- Another driver, name
- Boat Number
- The Results as posted

## **SECTION 4**

Which rule has been infringed:

Time of incident:

On which lap was the incident:

Where was the incident:

## **SECTION 5**

Your signature

Time of signature

**SECTION 6**

You must see this section completed by the official receiving this Protest:

Name of Official:

Time Protest received:

**PART B**

**SECTION 7**

Either, explain incident with another driver; in writing and by drawing;

Or, explain your reason for Protesting against the Race Committee:



## **500 - TECHNICAL RULES, CLASSES**

### **500.01 - GENERAL**

The group rules contain general technical information.

By word “boat” it is understood to mean any vessel used in powerboat racing.

When a matter is strongly recommended, the driver (pilot) choosing a different solution may be asked to explain to the measurer or scrutineer the advantages of his solution.

### **500.02 - INTERNATIONAL OFFSHORE CLASSES**

All classes for which the rules have been drawn up by THE NATIONAL AUTHORITY are International.

### **500.03 - RECOGNISED CLASSES**

All national classes for which the rules have been properly established by a National Authority.

## **501 - MEASUREMENT CERTIFICATE**

A boat is not allowed to take part in any race without a measurement certificate, lodged and approved by UKOPRA.

### **501.11 - HULL MEASUREMENT**

All hull measurements are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external moulded length of the boat, taken at the extreme bow and the rear most planning surface (the rear most part of the keel) including all hull planning steps (regardless of height), but excluding trim tabs, either fixed or movable.

Hull extensions beyond the transom shall be deemed to be “fixed trim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, outboard motor spacing brackets, stabilising trim tabs and rudder assemblies are not to be included in the measured length.

Trim tabs, either fixed or moveable, shall not exceed a length of 10% of the measured length of the hull.

Multihull tunnel flaps shall not exceed a length of 10% of the measured length of the hull.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Should the trailing edge of the planning surface of a hull or sponson terminate with an angle greater than 0 measured between the keel and outer chine in plan view (birds eye view), such excess shall not be included in the measured length.

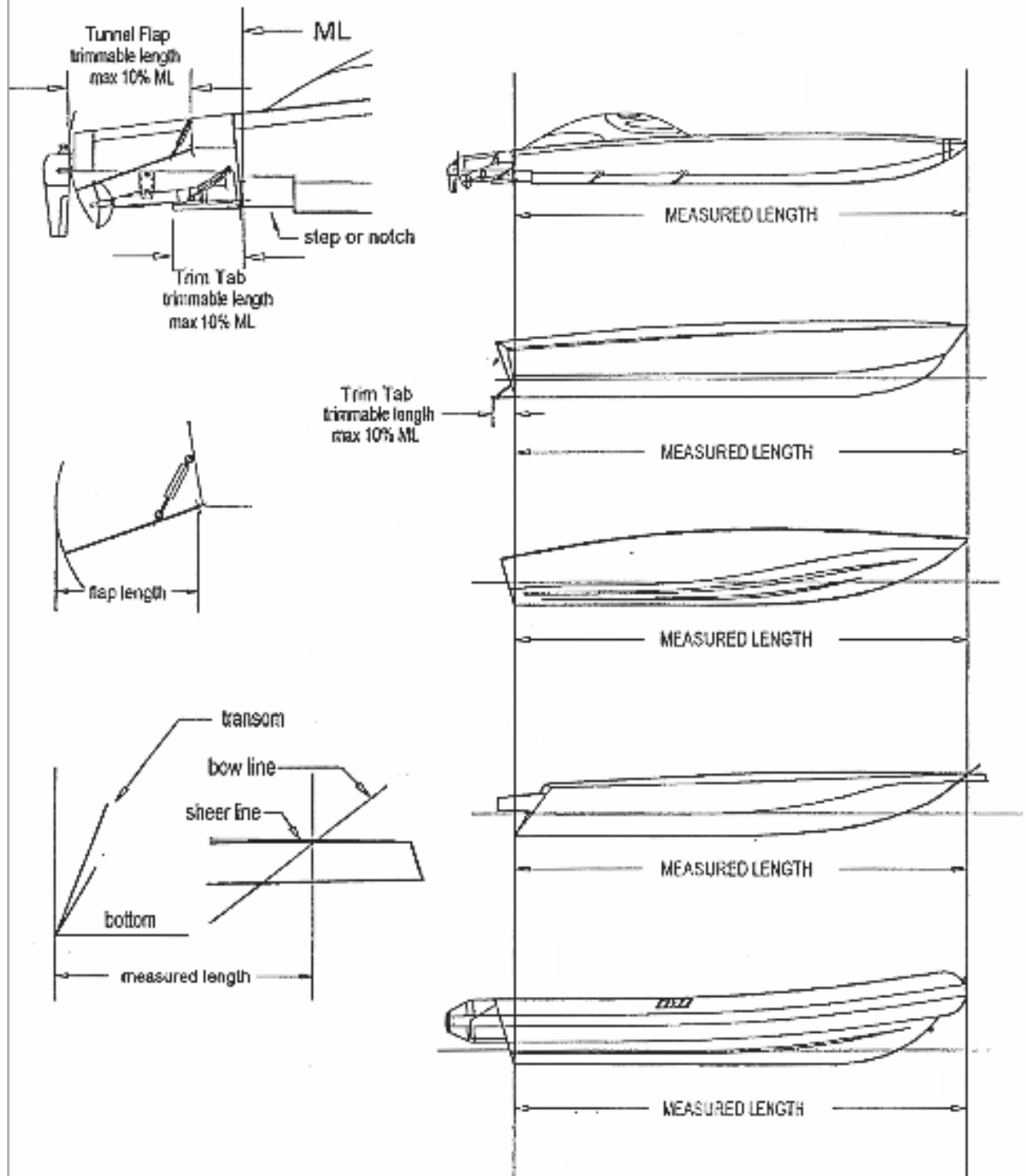
The beam measurement shall be taken at the widest part of the moulded hull, excluding any extending parts, rubbing strakes and/or fenders.

For Rigid Inflatable Boats (RIBs), the length measurement shall be taken at the extreme of the bow tubing (or rigid hull, whichever is the longer), to the rear most planning surface, excluding any trim tabs as for moulded hulls.

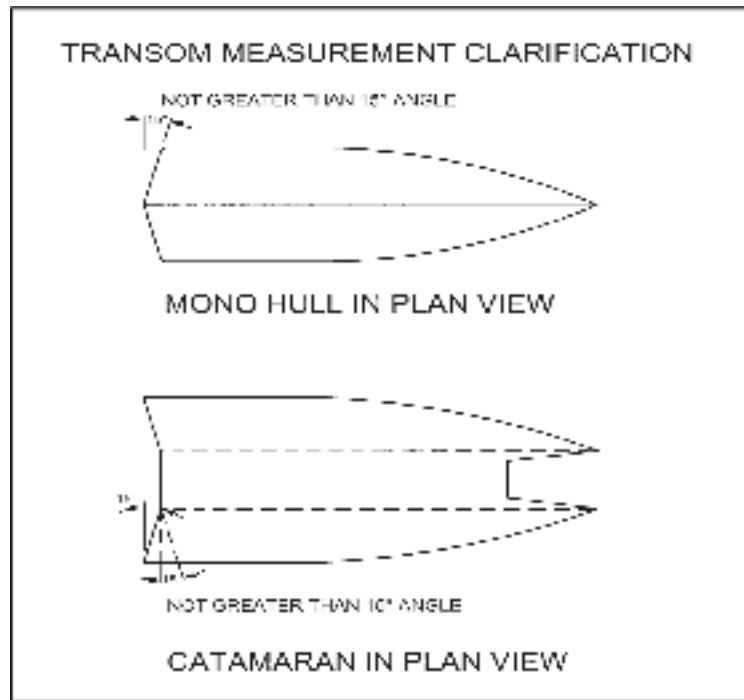
Tube extensions on RIBs extending beyond the rear most planning surfaces shall not be included in the measured length.

The RIB beam measurement shall be taken at the widest part of the inflatable tubes when fully inflated.

## LENGTH MEASUREMENT DIAGRAM



OFFSHORE UIM RULES nos. 501.11, 721.1



Description of the flotation (type, place(s) and volume) should be presented in the measurement certificate.

## 501.7 - FLOTATION

The owner may not change his flotation gear without having the corresponding changes noted in the measurement certificate, by an official measurer. However, additions to the certified flotation are allowed.

## 502 - INSPECTIONS

### 502.01.01

Hulls, motors, fuel, accessories and equipment, subject to restrictions regarding dimensions or other characteristics, must be submitted for verification.

At every race meeting the boats must be inspected before the race and practice due to safety reasons.

### 502.01.02

Drivers are at all times responsible for the condition of their boat (hull, motors, accessories, equipment, etc.)

### 502.01.03

Errors, if any, on the part of the manufacturer, builder, mechanic or even the previous owner do not justify in any way non-conformity with the rules.

#### **502.01.04**

Any driver refusing to comply with the decisions of the Technical Inspectors or who does not comply with the conditions of the rules, is not allowed to take part in a race or, should he have raced, cannot be classified and penalties may be applied.

#### **502.01.05**

Technical inspectors have the right, once races are over, to carry out all the checks they consider necessary, even when inspection has taken place before practice, they have the right to inspect as they think fit.

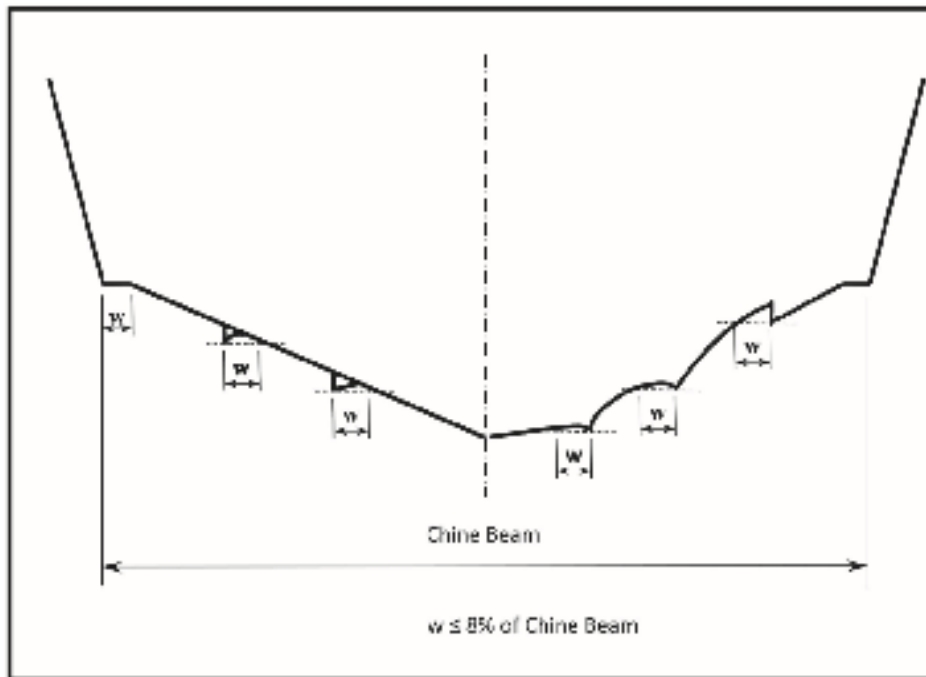
#### **502.02 - WINGS**

A wing is defined as a device above the deck that has a downward plus or minus 0 degree lower surface, with an area in excess of 0. square (.0 sq. ft).

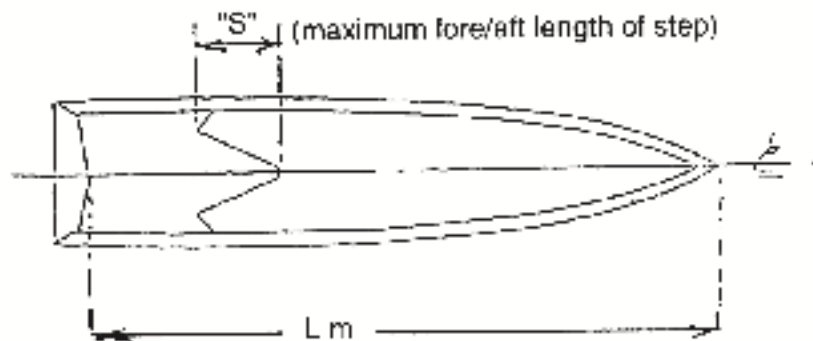
***Wings are not permitted.***

#### **502.03 - MONOHULL DEFINITION**

1. A monohull should be a boat with one hull.
2. A minimum distance of 0% of the "Measured Length" should, in the centerline, be the deepest part of the hull.
3. Transverse sections should, from the centerline, have positive angles up to the sections maximum beam, except in the following two cases:
  - (i) Each spray deflector may have a concave and/or negative angled surface with a horizontal width from the outside edge of the spray deflector in to the hull of no more than 8% of the hull's maximum chine beam (see drawing).



(ii) Steps which are swept aft or forward may have a distance "S" of no more than 2 % of "Measured Length" (see drawing)



**502.04 - ENGINES****502.04.01**

The total cylinder capacity/**power** of the motor or motors forms the basis for eligibility in the corresponding classes. The engines may be of the two-stroke or the four-stroke types.<sup>[L]</sup><sup>[SEP]</sup>Fuel may be fed to the engine by a carburetor or by injection.

**502.04.02**

Rotary motors of the type covered by the NSU-Wankel patent are admitted on the basis of an equivalent cylinder capacity.

The rotary motor cylinder capacity equivalent is twice the volume determined by the difference between the greatest volume and the least volume of the working chamber.

**502.04.03**

Verification of motors are carried out according to the facts contained in the homologation file.

At World records runs, World Championships and Continental Championships, inspection of the winning motor(s) is compulsory.

Inspections may also be carried out whenever necessary at all International and National events.<sup>[L]</sup><sup>[SEP]</sup>The following procedure is mandatory for these inspections:<sup>[L]</sup><sup>[SEP]</sup>. Check that the motor type code and visual appearance conforms to the motor homologation sheet.

2. The measurements stated in the homologation sheet must be checked on the motor. If the results are within the specified tolerances, the motor should be accepted as legal. Only adjustments to meet dimensions shown in the homologation sheet are allowed.

3. When ports in cylinders are adjusted to the dimensions specified in the homologation sheet, material may only be removed in the specified opening to a depth of 10mm, to match the shape of the original adjacent connecting passage (channel) outside the adjusted port opening.

4. Parts not specified with measurements in the homologation sheet can only be checked by comparison with standard parts. As the actual manufacturing tolerances are not published, small differences between the measurements of the inspected part and the reference standard part must be accepted.

5. Cylinder dimensions are measured when the motor is cold. Allowance is made for the carbon and oil present in the combustion chambers. Checking is done with oil. A large scale graduated cylinder or burette with adequate orifice is to be used.

#### 502.04.04

At anytime during an event, the technical inspector, the O.O.D. or the UKOPRA Commissioner reserves the right to change a competitors existing ECU/CDU unit with a standard OEM unit as supplied by UKOPRA

UKOPRA race officials must allow each competitor sufficient time to change the ECU and check that it is working. UKOPRA scrutineers will supervise the installation and seal the units on completion. UKOPRA ECU units will be removed post- race and/or the supervision of UKOPRA scrutineer. This process is mandatory.

If a competitor refuses this request, they will be immediately disqualified from the event, without right of protest.

### 503 - EQUIPMENT

#### 503.01

During a race, the boat must have onboard all the equipment required by the Race Committee in the detailed instructions contained in the advance programme.

#### 503.02

All attachments, handholds, steering gear, pulleys, fair leads, cleats and anchor bits, etc., must be bolted to the hull.

#### 503.03

Hatches and covers are to remain closed during racing, except for temporary checks.

### 504 - FUEL

N.B. Although International standards exist for petrol and for diesel, there remains a huge imbalance of specification from one country to another regarding the specification of 'pump' fuel available.

Due to these circumstances no specification of fuel is illustrated in the following rules.

Organisers are encouraged to **specify** a common fuel **and its supplier or supply such common fuel** for use at International events in an effort to maintain equal opportunity for all competitors.

***It is a requirement that all boats use such common fuel. Testing should take place and boats found not to be using the common fuel will be disqualified***



#### **504.01**

The only fuel to be used by any offshore powerboat must have the same characteristics as the fuel commercially available to the general public as dispensed from roadside pumps or marine fueling facilities.

#### **504.02**

For petrol engines, the fuel used shall be unleaded petrol.

No additives are permitted except lubricating oil for two stroke engines, providing that such oil does not increase the octane or the water content of the fuel or enhance the fuel by any other means.

#### **504.03**

For diesel engines, the fuel shall be that intended for use in diesel engined road vehicles or automotive based marine engines (including red diesel).

#### **504.04**

Post-race comparison testing may be used to compare petrol fuel samples taken from boats, to that of the original supplied fuel or a locally obtained sample if the fuel was not supplied by the organiser.

Post-race testing shall check:

1. Oxygenates by checking the electrical conductivity using the Digatron DT-apparatus.
2. Specific Gravity by using a Petroleum Hydrometer.
3. Water Solubility for detection of Methanol and other water-soluble additives.
4. Ceric Nitrate re-agent test for the detection of Alcohol.

#### **1. Oxygenates**

##### **Use of Digatron DT-15**

This apparatus is to be used for comparison testing ONLY.

20 7 Offshore Rules

##### **Comparison of Petrol/Petrol with two-stroke additive**

##### **Comparison of Petrol/Petrol (no two-stroke additive)**

- Stabilise the electrode of the Digatron by immersion in the 'bench mark' fuel.

- Set the indicator to Zero.
- Competitors using two-stroke oil must declare the brand of oil and the ratio of oil used to the CTO. (Take firm note of 0 .02)
- A sample of the mixture must be made using a small quantity of the 'bench mark' fuel and the declared oil.
- The 'bench mark' fuel (now containing the appropriate oil) can now be compared to that of the competitor sample.
- The procedure for comparison testing is as per the petrol/petrol comparison.
- Remove the electrode and clean off any residue of 'bench mark' fuel. Wait a few moments for any final residue to evaporate.
- Immerse the electrode into the competitor fuel sample and observe the reading of the indicator.
- It is very unusual to find an identical reading. (This is due to influences of differing temperatures, aeration, etc. of the sample)
- An indication of between -10.0 and +10.0 is normal and indicates a satisfactory comparison.
- An indication exceeding these parameters is justifiable reason for laboratory analysis.

## 2. Specific Gravity

The use of a Petroleum Hydrometer should be made to compare the specific gravity of the 'bench mark' sample against the competitor sample.

Both readings should be identical. The temperature of both bench mark samples and competitor sample should be the same.

## 3. Water Solubility

This test should be conducted as per the text of the Digatron instructions.<sup>[1][2]</sup>Both 'bench mark' sample and competitor sample readings should be identical.

This test should be conducted as per the Digatron instructions.<sup>[1][2]</sup>Both 'bench mark' sample and competitor sample results should be identical.

## **504.05**

### **4. Ceric-Nitrate re-agent**

If the results of the above tests show characteristics of similarity between the locally obtained sample (or fuel supplied by race organiser) and the sample taken from the competitor, then the fuel must be accepted as legal.

A judgment of illegality cannot be made on the results of the above tests alone.

Only the results from a laboratory analysis can be used to declare a fuel conclusively illegal or not.

If the results of the above comparison tests do not show characteristics of similarity and give cause for doubt, then the following detailed 'Fuel Sample for Laboratory Analysis' procedures shall be enforced.

### **504.06 - FUEL SAMPLES FOR LABORATORY ANALYSIS Procedure**

Fuel samples for laboratory analysis may be taken at any time and place during any event under the authority of the UKOPRA such samples shall be taken at the discretion of the Chief Technical Officer and must be as per U.I.M procedure. Fuel Test Certificates (in quadruple) shall be made available to the Technical Officers responsible for collecting fuel samples.

Only nominated Technical Officials may take samples.

## **504.07**

At the beginning of the event, the Chief Technical Officer must obtain sufficient fuel from the local fuel source (or the fuel supplied by the organiser) for comparison test sampling.

This fuel will be referred to as the 'Bench Mark' fuel and will be the fuel to which all comparisons will be made.

## **504.08**

It is mandatory to carry out fuel testing on the heat and/or race winners (to third place) at all World Championship events and recommended at Regional Championship events.

In a multi-race Championship, the fuel testing may be carried out by testing or more boats at each race.

**504.09**

Any competitor refusing to provide an adequate fuel sample or having insufficient fuel available in the boat's main fuel system or tanks immediately following completion of the race, will be disqualified.

**504.10**

Race Organisers must ensure that there is a supply of at least six one litre containers per class of boat competing for each heat/race of the event.

Such containers must:

1. Be clean and constructed of robust, fuel non-reactive, impermeable material
2. Be sealable
3. Have provision for identification.

**504.11**

Equipment used for the extraction of fuel from boats must be clean and constructed of non-reactive material.

**504.7**

Each sample must be divided into two and placed in separate one litre containers, being sample A and sample B. The containers must be completely filled up and immediately sealed and identified to the boat from which the sample was taken. This information must be entered on the Fuel Test Certificate and on the sample containers. The Fuel Test Certificate must record; the place and time of taking the sample, the identity of the boat from which the sample was taken and the identity of the driver of the boat.

**504.13**

Sample A must remain in the control of the Chief Technical Officer or his deputy and sample B must be given to the driver or a representative of the driver's team. The driver or the representative must sign the Fuel Test Certificate, acknowledging receipt of the sample.

**504.14**

A sealed sample of both the competitor's fuel and the benchmark sample must be sent to an authorised petroleum laboratory for full analysis.

The Chief Technical Officer must deliver all such samples to a courier authorised by the Organising Committee or the UKOPRA Commissioner.

The C.T.O. must return a copy of the Fuel Test Certificate, signed by the courier for the receipt of the samples, to the Organising Committee.

The authorised courier must deliver all "A" samples, together with copies of the relevant Fuel Test Certificate to the nominated laboratory, where they must be tested in accordance with standard International scientific procedures.

If the result of the laboratory analysis proves that the fuel is not within the same characteristics as the benchmark sample, the analysis must be paid for by the competitor. If the fuel is within the same characteristics as the benchmark sample, the organiser must pay for the analysis. The same rule shall apply for testing diesel.

**504.15**

The results obtained from such testing must be attached to the laboratory's copy of the Fuel Test Certificate and delivered to the UKOPRA as soon as practicable after the results have been obtained.

**504.16**

UKOPRA must as soon as practicable after receipt of the results, notify the following:

1. The relevant drivers or team representative
2. The Race Organiser.

**504.17 - AIR**

Only air may be mixed with the fuel as an oxidant.

## **504.18 - DEFINITION OF PETROL**

Petrol described within these rules is defined as one of the following:

1. Petrol of a kind recognised by the UKOPRA as being on general and genuine sale to the public in a country where UKOPRA International events are held annually.
2. The fuel used in Offshore racing shall be pump petrol as this term is generally understood. The detailed requirements of these rules are intended to achieve this purpose whilst allowing the use of absolutely consistent petrol's for racing purposes.

Any petrol which appears to have been formulated in order to subvert the purpose of this regulation will be deemed to be illegal.

Fuel suppliers are invited to supply samples of their petrol's to be checked for conformity before use.

## **505 - ENGINE HOMOLOGATION**

Manufacturers Engine Homologation data will be adhered to in Class 3 where using stock engines.

## **508 - CREW SAFETY**

All mono / multi hulls must be equipped with a Reinforced Cockpit(s) with Canopies for all riding crew members and buoyancy to ensure the boat floats.

The crew, who must be seated, must have a restraint system comprising of and conforming with the following rules:

A Reinforced Cockpit with Canopies is defined as a containment area for crew and can be constructed as an integral part of the boat.

This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat, and therefore protecting all members of the crew in the event of an accident.

The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanism, emergency air supply and restraint systems.

It is recommended that Sponson Cockpits are not used.

These rules also apply to any boat in any class using Reinforced Cockpits with Canopies.

***Boats with reinforced cockpit are not allowed to be driven faster than the maximum speed set by the designer specified in the closed cockpit registration. Any boats found to be driving faster than their maximum speed will be penalized up to disqualification.***

#### **508.01 - COCKPIT EVACUATION / IMMERSION TRAINING**

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion training in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the Immersion training, all crews must have a valid scuba certificate or have received suitable training.

An immersion Certificate to certify the passed test, showing the expiry date, must be delivered by Experts.

All riding crew members using restraints must sign the UKOPRA indemnity form prior to competing in any race or practice.

#### **508.02 - DRAWINGS AND MEASUREMENT**

Three view drawings (plan, side and elevation) of the design of the Reinforced Crew Cockpits(s), the Bulkheads, the type of Canopy, the Buoyancy System and the Restraint System anchorage Points must be lodged with UKOPRA of the measurer and verified at the time of craft measurement.

Drawings shall be provided showing canopy aperture dimensions for full or partial canopies, single or tandem arrangements. Arrangements shall describe whether fore and aft, or side by side seating is fitted.

Drawings shall show the method and construction of release devices. Drawings should show the material specification of the transparent areas.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement.

#### **508.03 - REINFORCED COCKPIT AREA AND CANOPY**

The reinforced cockpit(s) shall be of a closed type design with a minimum of one opening hatch and constructed to a similar strength as the running surface of the boat. This area must be the sides, floor, decking and bulkheads fore and aft.

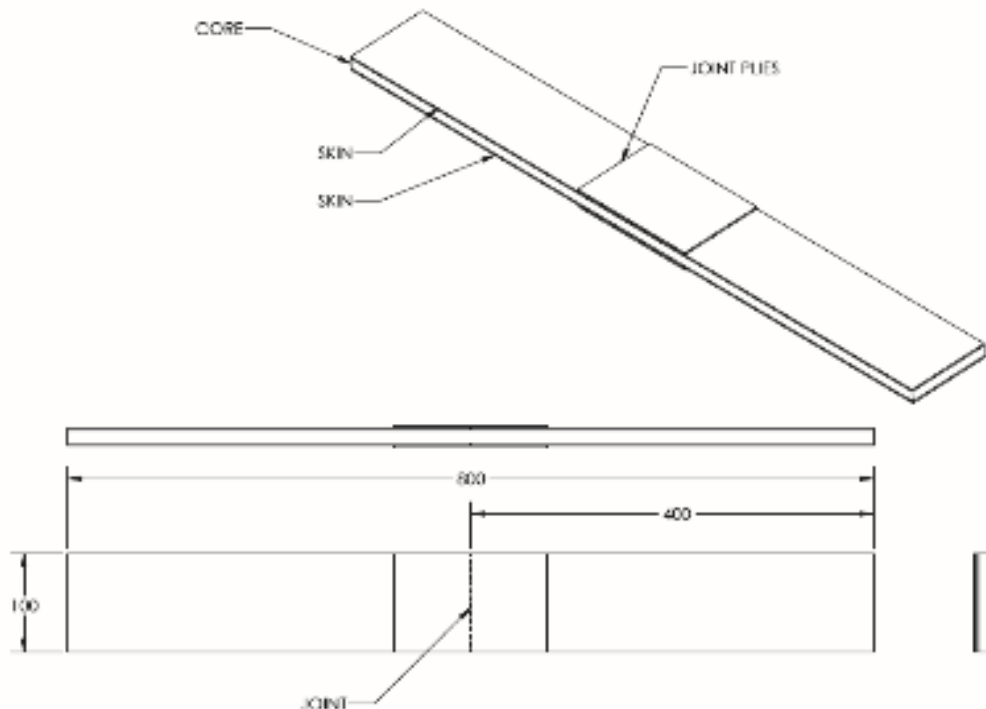
All new cockpits built after January 1, 2015 shall be built by a UKOPRA registered cockpit builder. Cockpit builders wishing to be registered must submit panels for test

according to the following standards. Cockpit builders meeting these standards will be registered as UKOPRA registered cockpit builders.

## UKOPRA OFFSHORE COCKPIT/CANOPY TEST STANDARD

### Sample Construction Requirements

a. Sample quantity must be, one of which must include a sample of the joint/bonding of the cockpit parts, transverse to the length of the sample.



b. Trimmed sample size must be 100 mm  $\pm$  1.0 mm wide x 800 mm  $\pm$  5.0 mm, with the width being parallel.

c. For fibre orientation the 800 mm length is to be parallel with the centerline of the boat.

d. Sample must be laminated on a flat surface using the same manufacturing process, materials, and fibre orientations as the intended homologated cockpit construction.

e. The sample must have a uniform thickness with no core crushing along any edges.

f. The sample must have one moulded face and the other face being unmoulded, the moulded face will be taken as being the external surface of the cockpit during testing.

g. The sample must be representative of the thinnest lay-up of the cockpit/canopy (excluding the various flanges for windshields, hatches etc.).



- h. The sample and cockpit must be manufactured using balanced or unbiased materials.
- a. The sample will be supported across the full width perpendicular to the 800 mm edges by two parallel 2 mm Steel bars at a distance of 500 mm apart. The load will be applied equally through two 2 mm Steel bars, each a distance of 7 mm parallel from each support.
- b. The moulded face of the sample will have the load applied and the unmoulded face will support the sample.
- c. The load will be applied at 0.4 mm/sec and the deflection will be measured at the two 2 mm Steel bars applying the load within 2 minutes.

### **Sample Test Requirements**

- a. The sample when loaded with the force required for the Class must have no more than a maximum deflection of 2 mm without the sample failing.
- b. The sample weight in gm/sq m will be calculated, skin thickness and sample thickness will be measured to enable inspection and comparison of damaged homologated cockpits/canopies.
- c. Further non-destructive test analysis methods may be used to compare test samples with homologated cockpits during the life of each cockpit/canopy.

### **Sample Manufacturing Information Requirements**

- a. Ply laminating sequence (stating which ply is the moulded face).
- b. Ply materials.<sup>[[SEP]]</sup>
- c. Ply weave styles.<sup>[[SEP]]</sup>
- d. Ply material weight in gm/sq m (dry weight ie. Without resin)
- e. Ply orientation (where 0 is parallel with the 800 mm edges).
- f. Core material and density in lbs/cu ft or kg/cu m.
- g. Manufacturing method (stating vacuum, pressure, and temperature).
- h. A 100 mm x 100 mm sample of all materials used (resin samples not required)
- i. The completed questionnaire for offshore cockpits (available on the UKOPRA web site) along with the supporting analysis for the question on "Primary Structure Strength"

Samples as per sample construction requirements must be sent to the UKOPRA appointed person/company.

For classes 3-225 , C, and D the minimum test standard shall be 3000 Newtons force. For XCAT and Class V the minimum test standard shall be 10000 Newtons

force. For Class 1 the minimum test standard shall be 20000 Newtons force.

#### **508.04**

Canopies must be a composite structure with the following features.

#### **508.05**

Polycarbonate areas are strongly recommended to be as small as possible while still maintaining that the driver and co-driver have clear, safe and undisturbed visibility ahead at sea level whilst racing. For Class it is strongly recommended that these polycarbonate areas are built using 2 mm thickness, or more.

The combined visibility of driver and co-driver must be through a horizontal arc of 22 degrees ( 2. degrees either side of the centre line of the boat).

These polycarbonate panels are to be recessed into the composite structure and may be bonded using a suitable bonding agent, and/or “bobbins”.

It is highly recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.

#### **508.06 - SCREEN FLANGES**

Screen flanges shall be a minimum of 50 mm at forward direction and mm towards sides and should be fastened every 100 mm if using “bobbins”; it is recommended to use metal “bobbins” with heads, as opposed to the recessed plastic type.

The outer polycarbonate area of the flange fitting must not be painted, so that the measurer/ scrutineer may monitor any discrepancies.

- Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.
- Bolts: min mm stainless steel, nylock nuts, washers.
- Bolt spacing: max. 10 cm if not glued
- The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 0 mm min) to prevent water forcing open the hatch in the event of a capsize.

**508.07 - ROLL BAR**

These Restraint Cockpits must be fitted with an internal roll bar, two in a tandem cockpit as a minimum. There must also be, between the two single cockpits, an anti-compression strut or structure of similar strength to the roll bar.

- Roll bar in front of/around each crew member.
- Roll bar strong enough and well secured to the bottom stringers.
- Central compression strut to hold roll bar, for side by side cockpits. Side compression struts may also be necessary for side by side cockpits.
- Alternatively, instead of a compression strut, the design of the cockpit primary structure will consist of a center roof rib connected to the roll bar and the aft bulkhead with sufficient strength to satisfactorily react the design impact loads.

**508.08 - HATCHES**

Hatch openings shall have a minimum of 2 mm flange.

Hatches must have a slot for pry bar, on the opposite side of the hinges, use in emergency/rescue.

**508.09 - HATCHES**

Hatches should be recessed on the front and sides.

The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsized.

Water deflector to be fitted only on front and sides of hatch, not behind of hatch. (A water deflector on back of hatch might force water into cockpit area.)

**508.10**

It is mandatory that the hatches are constructed to the same specification as the cockpit. The hatches shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These hatches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary.

These hatches should be fitted with hinges with short release pins. This is important because long pins invariably bind the hinge.

There should be one or more divers grab handles fitted to the outside of each hatch.

**508.11 508.7**

Canopy hatch release handles, which must be provided both inside and out, must be painted fluorescent orange or have a fluorescent orange background panel to identify them and directional arrows to indicate the method of opening.

**508.13**

The canopy lid hinges and the canopy hatch covers release mechanism must not encroach within the canopy aperture area, and these hinges and release mechanisms must not in any way hinder the exiting of crew members when fully race fitted.

**508.14**

Canopy openings should have the entry/exit apertures located directly above the crews' heads.

The canopy aperture openings should be at least 0.55 m in length and 0.55 m in width. If the crew is seated side by side, then the opening should be at least 0.55 m x 0.825 m wide. In tandem configuration, the opening(s) should be 0.55 m x 0.55 m per crew member. The canopy apertures should be cut with all corners having a radius of 0.02 m minimum or 0.2 m maximum. The radius should be constant and have a smooth finish to relieve stress.

**508.15**

The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening, both inside and outside of the opening.

**508.16**

It is mandatory that one single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 1 cm in diameter for easy reading.

Air supply bottles shall be "Turned On" before starting a race or taking part in practice and/or testing.

**508.17**

Reinforced Cockpits must have flood tubes or other means of flooding the cockpit to equalise the pressure quickly in an accident. The floor of the cockpit should be as air tight as possible to help the cockpit pressure equalise far more quickly when in an

upturned position.

### 508.18

Boats with restraints must have stop buttons/switches located in the cockpit area, immediately accessible to driver, co-driver and rescue officers. The stop buttons/switches must be identified by a fluorescent colour.

These switches must shut off all fuel pumps as well as the ignition circuit.

In the case of diesel boats, the stop control cable for the fuel injection pump shall be a non-sleeved cable, so as to eliminate the cable being able to bond in a fire.

### 508.19 - STROBE LIGHT

All boats shall have a White or Orange High Intensity Strobe Light fitted to indicate “coming off the plane” but not needing assistance. The strobe light must be able to be operated by the throttle man, and should be operated by the throttle man if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. When dual canopies are used, the light may be on or behind either one.

This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.

### 508.20

Cockpits with Restraints must be fitted with rear of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 7 % of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum of 0.2m vertical and lateral clearance between the canopy and each of the crewmembers when in the normal seating position.

### 508.21

The Restraint System must consist of a **6 point** strap harness and should utilise **belts with a minimum width of 50 mm** and grommets to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment. The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimise compression injuries under high “G” loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width.

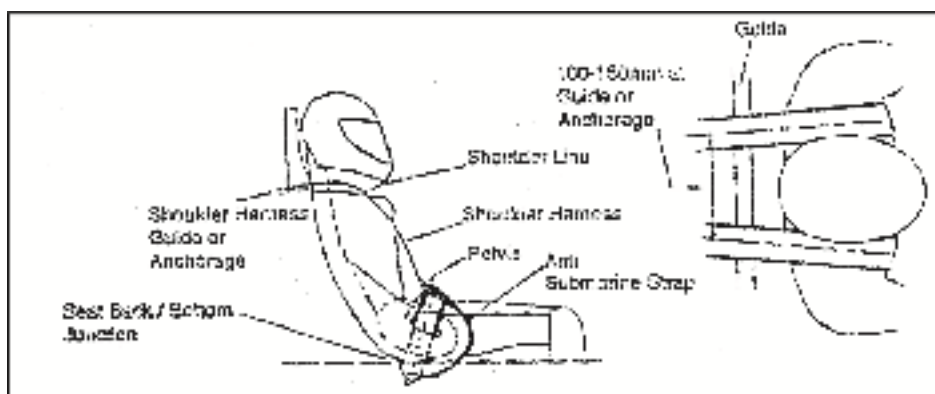
The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress. On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3mm thickness and 100 cm<sup>2</sup> area).

When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with UKOPRA of the measurer and approved prior to boat measurement.

All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems.

Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race. The harness system must comply with Drawing 2.

The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading.



7 mm minimum/maximum to Centre line of Lap Belt at Seat Back, Seat Bottom junction. Lap Belt should continue in straight line to anchorage.

## 508.22

A quick release steering wheel may be fitted on a boat with personal restraints, but all drivers must be able to exit the cockpit without removing the steering wheel.

## 508.23

Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst under way. Each wing mirror must have a minimum size of 60 sq.cm and be bolted on 2 points to assure proper mounting.

#### 508.24

For Class C, one extinguisher is sufficient.

Two fire extinguishers, each a minimum of 2kg, or of equivalent capacity, must be carried and be readily accessible to the crew.

The flares described in UKOPRA Offshore Rule 715.10 may be placed in a shallow locker adjacent to the deck race number.

Should a life raft be carried, it may be placed in the same locker. <sup>[17]</sup><sub>SEP</sub> All crew containment areas of inboard engine canopied boats must be fitted with a carbon-monoxide alarm.

#### 508.25

Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board, must wear a racing vest during the practice runs and throughout the race. Racing vests must be coloured high visibility orange or yellow. The racing vest must have epaulets/handles to help extract crew from the boat. The racing vest must have crutch straps or a method of ensuring that the vest does not “ride up”.

***The use of an inflatable racing vest is prohibited.***

#### 508.26

Each Reinforced Cockpit Area shall have one or more water activated light(s) or similar.

#### 508.27

All boats with restraints must have their bows painted fluorescent orange for at least 0.5m. Only boats with restraints and closed canopies are allowed to use orange coloured bows. If the hull is of a similar colour, then there must be a white separating band of at least 0.15m wide to ensure that the fluorescent orange band is obvious. If the number of riding crew exceeds two, the number of riding crew members must be written in black in at least 0.2 m high numbering on the orange nose in the following three locations:

- a) The lower running surface.
- b) The topside of the hull/sponson.
- c) The deck of the hull/sponson.

Should any boat be found to have contravened the riding crew number requirement, the penalty shall be disqualification from that event.

Note : Numbers shall be placed on the orange nose, not the white band area



### 508.29 - CLASS 3 CRITERIA

Clearance: helmet to hatch 10 cm min.

Cockpit construction:

- Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.
- Bolts: min mm stainless steel, nylock nuts, washers.
- Bolt spacing: max. 10 cm if not glued

### 508.28

It is recommended that the buoyancy should float the hull as parallel with the surface of the water as is practical, to help in rescue accessibility.

It is mandatory that sufficient buoyancy is provided in the boat, or in the material used for its construction, to ensure that the boat floats if capsized or holed. If extra buoyancy is needed, the buoyancy system described by the designer should be verified by the Measurer. This added buoyancy must be in at least four separate flotation units.

Each National Authority shall decide if its National racing shall be permitted to use Restraint Systems in Class boats below Class C.



**508.29 – CLASS 3 REINFORCED COCKPITS CRITERIA**

Reinforced cockpits are permitted in any category of Class 3. The following specification is the minimum mandatory standard for any reinforced cockpit used in any category of Class boat. See also rule 508.

Cockpit type:

The reinforced cockpit(s) shall be of a closed type design with a minimum of one opening hatch and constructed

to a similar strength as the running surface of the boat.

Cockpit minimum size:

Hatch opening per person: x 0 cm min.

Width: shoulder level 0 cm min.

Clearance: helmet to hatch 0 cm min.

Cockpit construction:

- Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.
- Bolts: min mm stainless steel, nylock nuts, washers.
- Bolt spacing: max. 0 cm if not glued
- The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 0 mm min) to prevent water forcing open the hatch in the event of a capsize.
- Hatches must have a slot for pry bar use in emergency/rescue.
- Controlling crew must have clear visibility ahead with adequate panoramic view.

Window areas:

- Material: Polycarbonate or similar.

Glass (in any form) is specifically prohibited.

- Min thickness of window :

Side by side cockpits min . mm.

Screens with curvature and/or tandem cockpit min . mm

N.B. With the exception of the above specific criteria in this section, the 08 rules

apply in full.

## **600 - OFFSHORE RULES**

### **601 - GENERAL**

Rule changes are only made following approval by. Such changes taking effect not less than six months after approval, unless a special decision is mentioned in the wording of the decision with its justification. Changes affecting safety may be made at any time, according to their necessity.

#### **601.01**

The following basic rules and definitions are for Offshore powerboats which have been designed and are suitable for racing in open and unprotected waters.

#### **601.02**

Organisers shall not be permitted to make any additions to these rules which by their nature would affect the eligibility of any boat EXCEPT in circumstances specifically affecting safety or in respect of special laws which may apply to the particular locality of the race. Organisers shall seek the approval of their National Authority to make such additions.

#### **601.03**

Organisers shall have the right to refuse any boat which they deem to be unsuitable and/or which does not conform to the requirements of these rules.

#### **601.04 - RESERVE NUMBER**

#### **601.05**

The finishing order is determined by the order in which the boat finish in its Class. Points for Championships are awarded separately for the Classes.

#### **601.06**

Organisers are free to organise prize categories for varying types, such as cabin boats, diesel engines etc., provided the craft comply with fundamental rules.

#### **601.06**

Organisers may also organise separate races for the different classes and/or prize categories.

**601.08**

Championships: Races included in championships are governed by individual rules of the particular championships as defined under their own rules.

**601.09**

All boats entered for racing shall be subject to the direction and control of the Race Committee.

**601.10**

It shall be the sole responsibility of each boat's driver(s) to decide whether or not to start or to continue the race.

**601.11**

Unless specified in racing instructions, no outside assistance (including refuelling) is allowed during a race.

The Race Committee has the right to decide whether or not there was outside assistance or not.

It is permitted to use electronic steering compasses and/or electronic position fixing equipment.

It is expressly forbidden to interface any of this equipment to the steering system.

**602 - CREW**

**602.1**

All boats will carry a minimum of two crew members with valid UKOPRA licences and of minimum 18 years of age.

**602.2**

Only licensed drivers are permitted to control the boat when under the jurisdiction of the race organiser.

**602.3**

Members of the crew under their age of majority must be required to produce evidence of parental consent to participate in the race.

#### **602.4**

Breach of the above rules shall entail immediate disqualification.

#### **602.5**

In championship races involving points qualification, the points will be awarded to the driver designated as the first driver on the entry form and who shall be aboard throughout the races.

### **603 – HULL**

#### **603.1 - DECK**

The deck must be able to bear the weight of a standing man 100kg at any point.

#### **603.2 - DECK, OUTBOARD MOTOR PROPELLED CRAFT**

Wherever the motor is, the hull shall be fully decked to the stern, incorporating a well formation if desired.

Any holes for the passage of cables, fuel pipes, etc., shall be fitted with sealing grommets and be above the lowest point of the stern structure.

#### **603.3 - STEERING POSITION**

The steering arrangements shall be sited so that the safe handling of the boat shall not be prejudiced by interference to the driver's line of sight across the bow at any state of trim in calm water.

#### **603.4 - GUARD RAILS**

Rails or wires or hand holes shall be fitted which shall extend fore and aft to enable crew to proceed from the cockpit to the whole deck for the working of mooring, anchoring and towage and access to deck hatches.

No ropes can be used as rails or hand holds.

#### **603.5 - MOORING CLEAT**

All boats shall be fitted with a well secured cleat or sampson post on the fore-deck adequate for anchoring in a seaway and for towing at sea over a prolonged period.

**603.6.1 - CRANAGE**

Boats must be fitted with a minimum of three lifting eyes, either through deck or topside hull cleat type, suitably engineered to withstand lifting the boat fully equipped and fuelled with an adequate safety margin.

Each boat shall be equipped with a detachable strop assembly providing a single lifting point. The strop must be certified to carry total boat weight.

**603.6.2 - CRANAGE SAFETY**

The Race Committee shall cordon off, restrict and supervise the crane area. Access to this area shall be limited to authorised race officials and designated race boat crews.

**603.8 - WINDSHIELD**

Windshields, if fitted, shall be strong and well supported. All edges of plastic material and framing shall be effectively padded. Transparent windscreens are to be made of polycarbonate material.

**603.9 - OPEN COCKPITS****603.9.1**

The cockpit opening shall provide seating positions for at least two drivers, with the following minimum dimensions; length 0.66m, width 0.53m per driver, with a round-off radius of 0.25 m.

**603.9.2**

There shall be a minimum cockpit clear depth of 0.38m everywhere, with the exception of seats, steering wheel and controls. Measurement is done from the upper part of the cockpit edge or from the top of the cockpit coming if it is substantial.

**603.9.3**

All cockpit seats must be securely fixed to the main hull structure. A seat for each crew member must be provided. The crew should be seated at all times when racing or attempting the race start.

**603.9.4**

The structure of cockpit fairings must at least equal the strength of the main deck i.e. be able to be walked on at any point as per rule 603.010

**603.9.5**

For Class A, only the overdeck water deflectors are mandatory.

The Reinforced Water Deflector must be designed and constructed of materials of sufficient strength to provide adequate crew protection, both over and under the deck and hull.

The forward fairing on the deck must rise to a minimum height of the chin of the tallest crew member when in the normal driving position. It is recommended that lateral protection covering be provided up to the shoulders. The 50 mm of the Water Deflector must be at least 45 degrees from the horizontal, with a minimum 300 mm width per person measured transversely in the horizontal plane.

The Reinforced Water Deflector must be designed and constructed so as to present no hazard if the crew is thrown forward and must be so designed that it would not restrict the crew from being ejected. In all cases, the visual requirements in 615.10 must be adhered to.

Measurers and Scrutineers will examine boats with the crew in the normal driving position.

The flares described in UKOPRA Offshore Rule 615.10 must be readily accessible from the deck.

Rear of head protection is recommended. If fitted, it should be at least 300 mm wide and the height of the padding must extend at least 6 % of the height of the safety helmet as worn by the crew whilst in the normal driving position.

If a monohull has a canopy and/or restraints, it must conform to the same criteria as multihull boats as described in these rules.

Water deflector, overdeck



**603.9.6.**

Only boats with restraints and closed canopies are allowed to use orange coloured bows.

**604 - INBOARD ENGINES**

**603.9.6**

In open cockpits, the cockpit edge or the type of a substantial coaming is not to be higher than the top of the crew persons' shoulders.

**604.1 - EXHAUST**

The engine exhaust manifolds, pipes and exhaust driven turbines, from the engine outlet to the point of exit must be water cooled by the water jackets or shielded. The external surface of the shield must not exceed 150 C at any time. The exit must be located in such a position whereby the crew cannot be effected by exhaust fumes.

**604.2 - SHAFT GUARDS**

Inboard propelled craft must have a protected shield to each input drive shaft which shall include at least two rings which completely encircle the shaft and a 180 degree scatter shield over each universal joint.

In the event of a failure this protection shall be capable of containing the shafts and couplings from causing damage to the hull skin, fuel tanks, any other installation and or component, and from causing any danger to the crew.

**604.3 - ENGINE COMPARTMENTS**

Inboard engines are to be installed in a compartment separated from the cockpit and other accommodation which is to have adequate ventilation. Vent holes in bulkheads or covers shall not be in close proximity to the drivers and crew without flame trap protection. The compartment(s) shall have rigid covers.

**604.4 - FIRE EXTINGUISHERS**

For all inboard engine installations, it is mandatory to have a fully automatic fire-extinguisher system, though it is permitted to have a separate manual system.



## **605 - ENGINES**

### **605.1**

Turbo and/or Supercharged petrol engines are not permitted.

When a diesel engine includes a device for supercharging or turbocharging, the nominal cylinder capacity is to be multiplied by; and the boat will pass into the class corresponding to the effective volume resulting from this multiplication.

### **605.02**

A dynamic air inlet for ducting the atmospheric air toward the engine's air intake is not considered a supercharging device.

### **605.3**

Turbo Compound engines and Gas Turbines are not permitted.

A Turbo Compound Engine is defined as a process in which effective power is produced not only by expansion in the working cylinders, but also in a second stage. In compound engines, the exhaust turbine, or one of the exhaust turbines, is coupled to the engine output shaft.

## **606 - ENGINE CUT-OUT**

### **606.1**

a) Engine cut off switches for connection to all crew are mandatory (first man out shuts off engine). An emergency override system to restart engines shall be mandatory. The lanyards used must not exceed 20 cm between the driver and the boat. The lanyards shall be attached to all crewmembers at all times when the boat is at racing speeds.

b) The lanyards must be able to support ten times the strength necessary to operate the device or to resist a load of ten kilos. Having chosen the biggest value, they must be capable of operating the device in any direction on a horizontal level.

c) Lanyards are not necessary when restraint systems are used.

d) If the engine is a Diesel, the action of the device should operate on the injection pump gear.

## **606 - ENGINE AND FUEL COMPARTMENT BULKHEADS**

### **606.1**

Bulkheads of engine and fuel tank compartments must be sealed to the hull across the bottom and have sufficient height above the bottom to prevent fuel and fumes flowing throughout the boat. The separate compartments thus formed must have provision for separate pumping out of bilge's.

Fuel containment systems (tanks, cells, etc.)

All boats must have fuel filling and fuel venting capabilities outside the crew containment area. Canopied craft are to have fuel tanks contained in sealed compartments from the hull and crew area to avoid leakage of liquid or vapour being released into the area being used by the crew. Fuel filling / fillers are to be outside of the crew compartments.

### **606.02**

Closing of the fuel flow must be done by means of a device, the control of which must be in the cockpit and within reach of every crewmember. No sleeved cables are allowed, so as to eliminate the cable being able to be bonded in a fire.

### **606.3**

The fuel tank or tanks must be grounded to enable the discharge of static electricity.

### **606.4**

Fuel tanks may be constructed as an integral component of the hull construction, or may be fabricated material suitably secured to the hull.

Purpose designed and constructed rubberised collapsible fuel cells may be used.

Fuel may not be transferred during a race except by means of permanently installed fuel lines connected to the tanks.

## **608 - PROPULSION**

### **608.1**

One hundred per cent of the propulsive effort shall be derived from the water while proceeding at racing trim in calm water.

### **608.2**

All craft must be able to be manoeuvred ahead and astern and have neutral capability, operated by controls at the helmsman's position. This facility must be a permanent installation and able to be demonstrated as practical at any time.

### **608.03**

Boats running two or more propulsion systems must have either an external tie bar to stabilise out drives or some other protective device that will prevent drives colliding should a failure occur.

### **608.04**

No multi-ratio transmissions, torque converters, or variable ratio drives are permitted in Class 3.

## **609 - GYROS**

### **609.01**

All moveable surfaces allowed on the boat shall be under the control of the onboard crew without any sensors in the control loop (i.e. no auto pilots, gyros, accelerometers or other attitude or acceleration sensors may be in the control system).

## **610 - RACE NUMBERS**

### **610.1**

All boats are to be allocated a race number by their National Authority. These numbers shall be painted in waterproof black paint or to be made of black coloured self-adhesive materials of adequate strength. They shall be applied to a white or yellow background. No number may begin with a zero. All numbers shall be plain and up- right.

## **610.2**

On all boats the numbers shall be positioned as far forward as possible whilst adhering to the requirements of rules 610.3 and 610.4

On catamaran boats the numbers must be placed on:

- a) The foredeck within front / of boat.
- b) The tunnel underside within front / of boat.
- c) The outside of both sponsons within front / of boat.

On all other boats the numbers must be placed on:

- a) The foredeck within front / of boat.
- b) Both sides of the hull within front / of boat.

## **610.3**

Those numbers displayed on the foredeck and on the underside of a tunnel must read correctly from the transom and shall be underlined by a black bar.

## **610.4**

The individual numbers shall conform to the following minimum dimensions, the only exception being where the size of the boat does not allow the minimum size to be carried:

Height 300mm

Width 230mm

Thickness 50mm

Spacing 80mm

The background shall extend at least 230mm in front of and beyond the end numerals and at least 65mm above and below the numerals.

The beam numbers may also be painted on slightly curved non-vertical surfaces, provided that the whole number is within 30 degrees from the vertical position. The height must be to the minimum height measured vertically.

**610.5**

Race Organisers are empowered to require a number to be changed in the event of identical numbers being issued by different National Authorities. Except in exceptional circumstances the visiting craft has priority.

**610.6**

The class letter will precede the numbers and they will be separated by a dash. The class letter must conform with the same minimum dimensions as the race number.

**Dimensions are as per 610.4**

**611 - NATIONAL FLAG****611.1**

All boats are to fly their appropriate national flag of the designated driver as determined by the entry, with minimum dimension 450mm X 300mm throughout the race.

**611.2**

Alternatively, the flag must be painted, or glued, on a panel of not less than 45m X 300mm on both sides of the hull.

## 67 - RACING VESTS

Racing vests (for boats without canopies). Each person aboard a boat must wear, during the race and practice, an efficient racing vest.

The racing vest shall be of full jacket type, with collar and leg straps. All straps shall be at least 40 mm wide and have a minimum breaking strain of 500 kg.

The vest shall be at least 60 % orange or yellow, have lifting straps at the front or on the shoulders and have impact protection material covering the back.

The disposition of the buoyancy must be such as to ensure that an unconscious person will float face up in the water.

***The racing vest's ability to float and rotate the wearer face-up must be checked by each individual wearer and personally ensured it is fully operational.***

The racing vest must have a minimum of 6.5 kg buoyancy.

The choice and efficiency of a racing vest is the sole responsibility of the wearer. Race organisers are required to repeat this important clause in the Advance Programme and the Race instructions.

For boats with canopies, refer to 508.25

## 613 - HELMETS

Any person aboard any boat partaking in races must wear an adequate helmet of orange colour throughout the race and during practice runs. Helmets may only be removed when effecting repairs and the boat is "off plane".

Each crew member's helmet must be identified with their boat race number.

This number shall be placed on the top surface of the helmet and shall be read from the rear. The numbers shall have a minimum height of 65 mm and shall be of the same style as the boat race number.

These numbers shall be painted in waterproof black paint or be made of black coloured self-adhesive material of adequate strength.

## **614 - PROTECTIVE CLOTHING**

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles.

Protective clothing used must be durable enough to provide bodily protection and it is recommended to be fire retardant

## **615 - SAFETY EQUIPMENT**

Whilst on the water at an event, all the relevant safety equipment, i.e., restraints, racing vests, helmets and racing suits must be worn whilst the race boat is on the plane.

### **615.01**

The following equipment shall be carried and stowed to the satisfaction of the Race Committee.

### **615.02**

Boats meeting the criteria of UKOPRA Offshore Rule 508 (restraints and canopies) must comply with the requirements regarding storage of safety equipment contained in those rules.

### **615.03**

When additional electric pumps are fitted to the fuel supply a fuel cut-off switch for the pump shall be fitted within reach of either crew member.

### **615.04**

A manually operated bilge pump with the capability of pumping out any one of the watertight compartments.

In addition to the above minimum it is highly recommended to have any additional electric bilge pumps that appear appropriate.

### **615.05**

Suitable gear for anchoring and berthing, including one anchor, not less than 0 metres of suitable chain or warp. Two towing lines of sufficient strength, one twice the length of the boat.

### **615.06**

Two (2) fire extinguishers to be immediately accessible to the crew, and must be

minimum of 2kg each, or of equivalent capacity.

For Class A, B and C, there is only a requirement for one fire extinguisher of 2 kg, or of equivalent capacity.

The extinguishers must fulfill the classification ABC (eq. US: classification AB).

American Class A Class B

European Class A Class B Class C UNCLASSIFIED Class D

Australian/Asian Class A Class B Class C Class E Class D Class F

Fuel/Heat source Ordinary combustibles Flammable liquids Flammable gases

Electrical equipment Combustible metals Cooking oil or fat Class F

Class C Class D Class K

#### **615.06**

An orange flag measuring at least 600mm X 400mm and the means of hoisting it, to indicate retirement.

#### **615.08**

A fitted steering compass.

#### **615.09**

Up-to-date charts covering the whole course of the race.

#### **615.10**

Two red hand-held distress flares and two orange smoke flares.

#### **615.11**

One whistle attached to each crew member.

One compresses attached to each crew member.<sup>[1]</sup><sup>[SEP]</sup>It is strongly recommended that one hand held flare is attached to each crew member.

#### **615.7**

A sharp knife suitable for cutting webbing, clothes or rope. The knife shall be suitably sheathed so as to protect crew members against accidental injury. It may be located in a fixed position in the cockpit adjacent to all crew members or, be separately carried on each crew member.



### 615.13

One diving-mask.

### 615.14

A pail or bucket with a minimum capacity of 5 litres.

### 615.16

Each Organiser can, in the Advance Programme, specify additional safety equipment necessary for their race.

## CLASS 3

### 630 - HULLS AND CLASSES

Offshore Class racing shall permit the use of boats powered by Outboard or Inboard engines.

#### 630.1

Class subdivisions are established in accordance with the following

#### UKOPRA Class 3 Criteria

Class	3A	3B	3X		3N
Cubic Capacity	<2100cc	<2100cc	<3000	<3500	No Limit
Max Power	115hp	115hp	200hp	300hp	As declared
Min Length	5.65m	5.0m	6.00m	6.65m	6.25m
Max Length	6.00m	6.8m	8.00m	8.00m	9.25m
Mono Min Weight inc crew	600kg	655kg	1000kg	Add 4kg/hp > 200	4.5KG / Declared HP
Multi Min Weight inc crew		630kg	Add 20%	Not Applicable	Add 20%
add KG/m		50kg/m			
Min Width	1.6m			1.60m	1.80m
Max Width (Monohull)	2.00m			2.40m	2.80m
Cube Length	2.35m	2.35m		2.65m	3.60m
Cube Width	1.20m	1.20m		1.20m	1.20m
Cube Height	0.46m	0.46m		0.46m	0.46m
Bouyancy					
Mono GPR	250L	260L		360L	500L
Mono Sandwich	130L	140L		150L	280L
Multi GPR		280L		440L	550L
Multi Sandwich		180L		180L	300L

For Class 3B A non stepped hull will be permitted to utilise 150hp / 2600cc but with a minimum weight including crew of 860kg.
Non homologated engines are accepted. HP rating is as that given by the manufacturer and engines should not be modified in any form from manufacturers specifications.
A lower rated base powerhead may be modified to bring it up to a declared horsepower that will be subjected to dyno testing if required.
3N requires a declaration of Horsepower to be made that will be subject to Dyno testing at the request of the scrutineer.
Minimum bouyancy requirements are not a guarantee that a boat will not submerge. Competitors are to make their own assessment as to suitability

In the 3A class, only monohulls will be accepted.

### **Hull measurement Class 3A and 3X**

All hull measurement are to be taken while the boat is ashore.

The length must be measured between perpendiculars of the external moulded length of the boat, taken at the extreme bow and the interface between the hull and bracket/engine.

Hull extension beyond the transom shall be deemed to be “fixed trim tabs” and shall not be included in the measured length.

Hull extensions added to the bow and/or any extending parts, rubbing strakes, fenders, stabilizing trim tabs and rudder assemblies are not to be included in the measured length.

When defining “extreme bow”, any form that constitutes the bow and its construction and contributes to the performance of the boat shall be included in the measured length. Any other attachment that is added to meet a minimum length requirement shall not be included in the measured length.

Boat width : **3A** . 0 m - 2.00 m. **3X 1,60 m - 2.10 m**

**631 - MINIMUM/MAXIMUM LENGTHS AND WEIGHTS**

The weight is ready to race dry, including all safety equipment **for all classes**. For control after a race, residual fuel in tanks will be included in the minimum weight. Slings carried in the boat during the race will count as safety equipment. The boat must be empty of water.

**For Class categories 3b** the minimum weight mentioned is for a boat with the mentioned minimum length. For boats longer than the minimum, the weight shall be increased according to the formula shown above.

**632 - HULL DIMENSIONS****632.1**

For monohulls, the outer surface of the hull excluding chine-rubbers and spray rails, must contain a cube of the dimensions mentioned in the 6 0. chart.

**633 - MINIMUM FLOTATION**

All boats shall contain a minimum volume of buoyancy permanently installed in the hull. The minimum volumes measured in litres are mentioned in the 6 0. chart.

If a boat is constructed of wood, the wooden weight content of the boat may be accepted as buoyancy as per the following formula: Wood weight in kg x 2 = litres of buoyancy.

**634 - ENGINE ELIGIBILITY****634.1 - ENGINE ELIGIBILITY (LOW EMISSION) OUTBOARD ENGINES**

Low Emission approved engines used in Class shall be strictly standard marine production engines as marketed by manufacturers to leisure boating (or touring) customers.

**634.2**

At time of homologation, manufacturers must certify that their motors meet the UKOPRA requirements regarding noise when fitted to an appropriate offshore powerboat.

The noise level is to be measured by means of a phonometer which conforms to the specifications laid down by IEC and ANSI, a precision sound meter using the "A" weighting and set to slow response.

The registering microphone is placed at a distance of 2 m from the boat. The boat

must pass it at top speed. Measurements must be made on open water free of all obstructions over a minimum distance of 2 m in all directions.

Two measurements must be carried out, while the boat approaches and while moving away. Neither of the measurements is to exceed db (A) with a db (A) tolerance.

### **634.3**

The only Low Emission outboard engines eligible for use in Offshore are those approved (at time of homologation application) by Cominoff.

**634.5** No. of manufactured engines up to:

000 cc = 000 units. 00 -2000 cc = 00 units. 200 cc and above = 2 0 units.

### **634.4**

The engine being placed in water, the cooling shall be effected only by its own standard circulation pump. The intake to the circulation pump may be modified for cooling purpose only. Bearing carriers and prop shaft may be changed from original. Bearing carriers may be fixed to gear case by additional screws or clamps.

Bearing carriers and prop shafts may be changed from original. Refer also to 6 .0 for water intake definition.

Violation of any of the above rules will entail disqualification.

## **635 - MODIFICATIONS PERMITTED (LOW EMISSION) OUTBOARD**

### **634.6**

## **ENGINES**

## **MODIFICATIONS PERMITTED (LOW EMISSION) OUTBOARD ENGINES**

### **635.01**

The propeller is free, together with its washer(s) and nut(s).

### **635.02**

The steering bar and brackets may be reinforced as a safety measure. It is also permitted to modify the standard engine cowling in order to facilitate the fitting of steering assemblies, provided that any openings are sealed so the engine cannot pull in any additional air.

**635.03**

The rubber mounts of the engine may be changed or substituted.

**635.04**

All external (not contained within the powerhead) studs, screws, nuts, bolts and their washers are free as well as the method of locking them, providing there is no performance advantage.

**635.05**

Where two engines are fitted, the counter rotation of one underwater unit is permitted providing: - the standard underwater unit casing is retained (subject to 635.09 the final propeller to crankshaft ratio is unaltered

**635.06**

The use of thrust blocks mounted on the lower unit and or on the boat is permitted.

**635.06**

Power Trim: a properly engineered system for altering engine trim whilst under way is permitted.

**635.08**

In the process of modification, minor optional alternatives as listed and priced by the manufacturer of the homologated engine, may be permitted provided they do not affect engine performance, but improve convenience and safety.

**635.09**

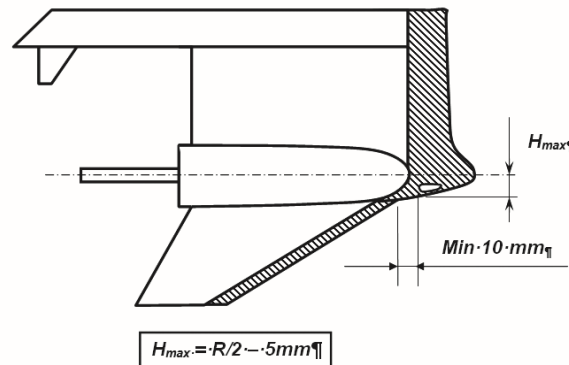
The water intake may be repositioned. It must however remain on the gear case torpedo. The width and or height of the gear case torpedo must not be changed and the depth of the torpedo cross section from the aft part of the water intake and forward is not allowed deeper than measure  **$H_{max} = r/2 - 5mm$ , there the measure  $r$  is taken from min width of gear case in homologation file**. The length may be increased. The rear edge of the water intake opening must not be further aft than 30 % of the total length of the modified gear case torpedo.

The original intake to the circulation pump may be filled in and faired. The front of the gearcase may have material added for cooling purposes.

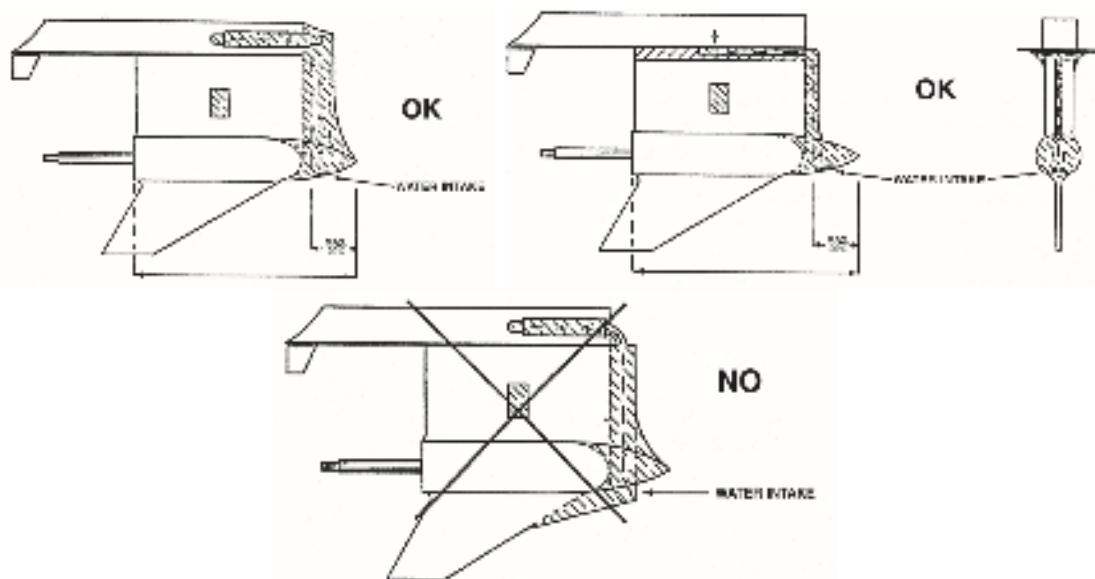
The width of the gearcase must not be increased, except where the gearcase meets the lower side of the anti- cavitation plate. The modification shall be for cooling purposes only and shall form a radius. It is also permitted to add material to the centre section exhaust housing for the purpose of reducing noise.

It is allowed to add material to the leading edge of the gear case skeg up to 10 mm behind a repositioned water intake. The profile of the skeg is free.

Remote water pick-ups are not allowed.



Method of measuring maximum depth of water intake.



## 635.10

Transom brackets may be reinforced, this is only by adding material.

## 635.11

- . a) The fuel connector in the lower cover may be removed and the fuel hose from the fuel tank connected directly to the fuel pump. The remaining void must be sealed to prevent any extra air entering the cover. [SEP]
- . b) Sacrificial anodes on gear house and transom bracket may be removed. Trim fin under the anti-cavitation plate may be removed or cut off. [SEP]

## **MODIFICATIONS NOT PERMITTED (LOW EMISSION) OUTBOARD ENGINES**

### **635.7**

The following modifications are expressly NOT permitted.

Blueprinting' of the powerhead.

Alteration or modification of any powerhead components.

Removal of material from the powerhead for any reason other than recognised (by the manufacturer) repair.

ECU/PCM's equipped with data memory, must not have the memory erased or modified without the prior permission of the Scrutineer.

Any information sourced by the Scrutineer from the ECU/PCM's race data memory which does not correspond with the technical data declared by the manufacturer in the homologation file will be viewed as reason for disqualification.

### **635.13**

Violation of any of the above rules will entail disqualification.

## **700 - RULES FOR UKOPRA MARATHON RACING**

### **1 - GENERAL**

#### **1.1 – RULES**

The general accepted offshore racing rules are to be applied. In cases of conflicts between the general rules and the class rules (and specifications), the class rules have precedence. (See UKOPRA General racing rules)

#### **1.2 - MINIMUM AGE**

The minimum age for competitors in all of the marathon classes is 18 years.

#### **1.3 - MEASUREMENT CERTIFICATE**

Measurement certificates are required for all classes.

#### **1.4 - CLARIFICATION**

In these Rules, the words “shall” or “must” means mandatory; the word “should” means recommended.

#### **1.4 – LICENCE**

The issuing of International Offshore licences is the task of UKOPRA.

The issuing body of the licence holder must verify that the competitors have the necessary experience and still to take part in Offshore Marathon events.

As a reference for minimum competence:

The Competitors who have not held a Marathon Licence must submit a detailed CV of their racing and boating experience to UKOPRA.

This CV must demonstrate practical experience and theoretical knowledge to a minimum Authority qualification or military equivalent these may include:  
RYA day Skipper – or RYA Coastal Skipper – or RYA Yachtmaster – or RYA Advanced Powerboat (not RYA Powerboat Level 2 on its own).

An International equivalent of qualifications will also be accepted.

Both driver and co-drivers must have first aid and resuscitation knowledge as defined by Red Cross or similar.

#### **1.5 - SEA SURVIVAL CERTIFICATE**

A current sea survival certificate must be held by all crew members.



## **2 - COMPETITION RULES**

### **2.1 – DECALS**

Competing craft will be asked to display the race decals on either side and on the deck of the craft.

The decals will be provided by the Event organiser's sponsors.

### **2.2 - OUTSIDE ASSISTANCE**

No outside assistance is allowed during a race.

Competitors may however refuel during a race at recognised port or marina fuel outlets within harbour confines.

Ship to ship or air to ship refuelling at sea is not permitted.

Any competing craft may be towed and if a tow is accepted, then that competitor will be treated as a retirement from the race or that particular leg of the race.

### **2.3 IDENTIFICATION**

All competing craft must carry race numbers as laid down in the Rules; this includes deck numbers.

Race numbers shall include a prefix letter, 'A', 'B', 'C', 'D', 'E', 'F', 'G' or 'H' to match each sub-class of Marathon classification.

Numbers shall be painted in waterproof black paint on white background or be made of black coloured self-adhesive materials of adequate strength.

No number may begin with a zero.

All numbers shall be plain and upright.

The numbers shall be painted as to be clearly visible from both sides and above. Numbers must be placed on both sides of the hull within the front third of the boat and on the foredeck within the front third of the boat.

Numbers displayed on the foredeck must read correctly from the transom and shall be underlined by a black bar.

Minimum dimensions for individual numbers must be:

Height = 30 cm; width = 23 cm; thickness = 5 cm; spacing = 13 cm.

The background shall extend at least 23 cm in front of and beyond the end number and at least 6.5 cm above and below the numbers.

British boats will be expected wherever possible to make a race number change where an overseas competitor has the same number.

## **2.4 – ADVERTISING**

No part of any advertising, sign writing, flags, badges, emblems or marks of any kind shall be within 60 cm of the race number.

No material may be displayed which advertises any product which is in conflict with the race sponsor's products unless prior permission in writing is obtained.

Display material considered to be offensive in the opinion of the organisers will be required to be removed. Failure to comply could result in disqualification.

## **2.5 - RACE FUEL**

Although International standards exist for petrol and for diesel, there remains a huge imbalance of specification from one country to another regarding the specification of 'pump' fuel available.

Due to these circumstances no specification of fuel is illustrated in the rules.

The only fuel to be used by any offshore Marathon powerboat must have the same characteristics as the fuel commercially available to the general public as dispensed from roadside pumps or marine fuelling facilities.

For petrol engines, the fuel used shall be unleaded petrol.

No additives are permitted except lubricating oil for two stroke engines, providing that such oil does not increase the octane or the water content of the fuel or enhance the fuel by any other means.

For diesel engines, the fuel shall be that

It is strongly recommended that teams keep detailed records of where they purchased the fuel they intend to use in races by way of receipts. In the case a sample is found to be illegal then a control sample can be sourced to demonstrate that the error is with the supplier, but the team will still be responsible and maybe disqualified, but might go some way to demonstrate it was not premeditated act.

(More details can be found by looking at UKPPRA rule 504)

# Offshore Powerboat Rules 2018

## 3 - RACE BOATS

### 3.1 - HULL LENGTH

The minimum hull length for all competing craft is 24ft/6.32 meters and the maximum for all is 50ft/15.20 meters using standard racing measurement procedure. See Class classification table in section 8.

### 3.2 – SPEED

All competing craft must be capable of exceeding 40 knots, except for Historic Class boats.

Historic Class boats must have a speed capability of 30 knots.

### 3.3 - MONOHULL BOATS

In the Spirit of Offshore Marathon Racing, all competing craft must be monohull

### 3.4 - CANOPIED BOATS

All competitors and crew members who race in boats with restraints, canopies, and partial canopies must hold a current immersion test certificate.

Canopied boats must have a current Measurement Certificate and comply with full Championship rules for canopies.

(See appendix 1 which is an extract of the 2016 UIM rule 508 for more details)

Additionally the following rules apply:

Boat builders/designers must confirm in writing that the boat is designed for such activities and to race in a given class. This confirmation is to be supported by detailed technical drawings as appropriate.

Crew must be able to demonstrate that they can safely exit the boat (maximum recommended exit time 30 seconds) and this may be subject to scrutineer testing at events.

The nominated skipper of the vessel accepts ultimate responsibility for complying with all of the above requirements.

Carbon monoxide sensors and/or alarms must be fitted in all canopied boats.

These rules are important safety rules and require adhering to, however certain rules may be slightly varied where a team feel there is a better safer alternative solution. If this is the case then a team should write to the Championship committee with the fullest written details and drawings.

If the committee in conjunction with the scrutineering team concurs to this requested variation then they may be accepted for use in the championship.

# Offshore Powerboat Rules 2018

## 3.5 - PARTIAL CANOPY BOATS

All competitors and crew members who race in boats with restraints, canopies, and partial canopies must hold a current immersion test certificate.

Partial canopies may be permitted subject to the technical inspector's approval, this process must be commenced a minimum of 3 months before the event.

There must be an opening hatch with a minimum open space sufficiently large (minimum 55cm X 82.5cm) for each person in the boat to exit immediately.

Alternatively, there must be an open space in the rear of the craft sufficiently large (minimum 1.3m X 1.3m) for all crew to exit the boat immediately.

Access at this opening must not be restricted in any way whatsoever.

Partially canopied boats must have restraint systems fitted which, if fitted, must comply with the following Offshore rules:

508.01 (Crew Immersion Test),  
508.16 (Air Supply),  
508.18 (stop buttons for engine cut-off),  
508.20 (Rear of Head Protection) and  
508.21 (Specification of 5 or 6 strap Harness).

Additionally the following rules apply:

- . Boat builders/designers must confirm in writing that the boat is designed for such activities and to race in a given class. This confirmation to include key safety points below, with detailed technical drawings supplied as appropriate.
- . Crew must be able to demonstrate that they can safely exit the boat (maximum recommended exit time 30 seconds) and this may be subject to scrutineer testing at events.
- . Seat belt mounts must be of appropriate strength and position, considering the boats maximum designed speed – see 508 rules.
- . Structure of the partial canopy must be of similar strength to the hull/running surface of the boat.
- . The screen must be of suitable materials and have flanges adequate to offer the strength required to meet the anticipated loads and speed of the craft.

# Offshore Powerboat Rules 2018

- . All crew must have in date immersion test training and hold a current immersion test certificate.
- . Doors or hatches must be so designed to allow them to be easily opened from inside and out and must be labelled to allow rescuer to immediately understand opening system and backup system – hinges must have removable pins.
- . There must be an air system provided for each crew member – spare air systems are not acceptable.
- . There must be a minimum clearance between seats or door aperture of 40cm if this is the primary exit route.
- . The nominated skipper of the vessel accepts ultimate responsibility for complying with all of the above requirements.
- . Carbon monoxide sensors and/or alarms must be fitted in all canopied boats.

These rules are important safety rules and require adhering to, however certain rules may be slightly varied where a team feel there is a better safer alternative solution.

If this is the case then a team should write to the Championship committee with the fullest written details and drawings.

If the committee in conjunction with the scrutineering team concurs to this requested variation then they may be accepted for use in the championship.

## **3.6 - SEAT BELTS/RESTRAINTS**

No seat belts or restraints whatsoever are permitted in open boats, i.e. boats with no canopies or boats with partial canopies.

## **3.6 - REINFORCED WATER DEFLECTOR**

ALL Craft with a top speed in excess of 50 knots which do not have a forward cabin structure must have a Reinforced Water Deflector over and under the deck, designed and constructed of materials with sufficient strength to provide adequate crew protection.

The forward fairing on deck must rise to a minimum height of the chin of the tallest crew member when in the normal driving position.

The top 5cm of the water deflector must be at least 45 degrees from the horizontal with a minimum of 30cm width per person measured transversely in the horizontal plane.

# Offshore Powerboat Rules 2018

The Reinforced Water Deflector must be designed and constructed so as to present no hazard if the crew is thrown forward and must be so designed that it would not restrict the crew from being ejected in all cases.

Open RIBs must have a solid fitted console to deflect water. In addition, all vessels must have a means of preventing the riding crew from sliding forward under the foredeck when in their normal racing position.

A bulkhead or suitable kick-board in front of each of the riding crew must be fitted and be of sufficient strength to prevent the riding crew from forward movement in the event of rapid deceleration.

The bulkhead/kick-board must be secured so that there is no more than 1 inch space between the crews' floor and the bulkhead.

## 3.8

All competing craft must be capable of going astern.

## 3.9 - BOW BALLAST TANKS

Bow ballast tanks are permitted.

## 3.10 - LIFTING POINTS

*Where physically possible*, teams should ensure that their boat has fixed lifting points, and their own certified lifting strop, which must be carried in the boat. For ALL boats, the lifting positions of the craft for emergency lift by crane slings must be clearly marked on either side of the boat.

## 3.11 – BUOYANCY

It is recommended that boats should have enough buoyancy in the race boat or in the material used for its construction to ensure that the boat floats if capsized or holed. Boats which do not have sufficient buoyancy may be refused entry to any competition by the organisers of each individual event if deemed unsafe by the organising committee or promoter.

## 3.7 - BILGE PUMPS

Suitable & automatic bilge pumps shall be fitted to the boat capable of pumping out all sections of the boat even where water-tight bulkheads are fitted. They shall be accessible and be fitted with a suction pipe leading to the lowest point of the bilge and with a discharge pipe overboard. There shall also be at least one manual bilge pump in the boat.

## 3.13 – PUMP

# Offshore Powerboat Rules 2018

All RIBS shall carry a hand or foot pump capable of being operated below the gunwale.

## **3.14 - DECK FITTINGS**

All boats must have guard rails or handholds as rails or handles raised above the deck of a suitable material or wire in stanchions. A single rail would suffice. A rope secured to the bow and made fast in the cockpit will not be accepted.

## **3.15 – TOWING**

All vessels must be fitted with a properly designed towing point of adequate construction and strength for the boat to be towed when waterlogged for extended periods.

A Tow line suitable in length and strength for the boat in all weathers must be carried by all craft and can be permanently attached to the strong point during races unless the crew can demonstrate that a towing line can be easily attached to the towing point whilst at sea.

This towing line should have a fixed loop to provide ease of attachment.

In addition each boat should carry sufficient fenders & warps as to be able to dock or raft independently in port or to be able to raft suitably to another vessel if assistance was either required or being offered.

## **3.16 – ANCHOR**

Anchor/s with adequate anchor lines must be carried at all times and shall be of a weight and type adequate to hold the boat and shall be properly stowed in an accessible place.

In addition for Ultra Marathon races all boats must carry a sea anchor (drogue) suitable the size and weight of the vessel.

## **3.16 – COMPASS**

Compass, Charts/back-up navigation and Nautical Almanac:

Compass - All boats must be equipped with a liquid filled magnetic compass.

# Offshore Powerboat Rules 2018

Compass deviation cards for magnetic compasses must relate to adjustment within the 7 months prior to the date of the event. Electronic compasses may be used in addition.

Charts/back-up navigation system – All boats must carry at all times:

*EITHER*

A comprehensive set of paper charts which must be in date and corrected to current Notices to Mariners.

These charts must be of suitable scales and detail to enable safe navigation over the entire course.

*OR*

A properly installed electronic chart plotter containing chart data of suitable scales and detail to enable safe navigation over the entire course.

In addition to the installed chart plotter a portable chart plotter containing chart data of suitable scales and detail must be carried to enable safe navigation over the entire course.

This additional unit must be provided with additional emergency batteries to ensure a minimum of 8 hours operation.

Nautical Almanac – All boats must carry at all times a nautical almanac or excerpts from a nautical almanac providing detailed navigational information of the entire course.

## **3.18 - NAVIGATION LIGHTS**



# Offshore Powerboat Rules 2018

Navigation lights in working order shall be carried in accordance with International Rules for the Prevention of Collisions at Sea.

## **3.19 - FOG HORN / TORCH**

All boats shall carry an efficient fog horn and a powerful torch.

## **3.20 - ENGINE CUT-OFF DEVICES**

Engine cut-off devices for connection to the crew are mandatory (first man out shuts off engine).

An emergency over-ride system to restart the engine/s shall be mandatory.

The lanyards used must not exceed 70cm between driver and the boat. The emergency cut-off devices must be positioned so that when they operate the lanyard and cap (or clip) will not catch or foul.

The lanyards shall be attached to all crew members at all times when the boat is racing.

For boats using restraints, see 3.5 above for stop buttons.

## **3.21 - FUEL TANKS**

Fuel tanks must be purpose made and permanently fixed.

They must be secure, non-leaking, vented, grounded and have an easily accessible means of shutting the fuel supply off from the tank/s.

For safety, it is strongly recommended that fuel tanks in engine compartments are suitably encased to prevent any flying objects in the engine compartments from penetrating the bare skin of the fuel tank.

When additional electric pumps are fitted to the fuel supply of outboard motors, a fuel cut-off switch for the pump shall be fitted in easy reach of all crew members.

The positioning of the fuel cut-off switch must be clearly marked for safety reasons.

Fuel transfer at sea is only permitted between your own fixed on board tanks. Race fuel may not be stored or carried on board in any jerry-can type of container.

## **3.22 – BATTERIES**

Batteries shall be housed in ventilated compartments, mounted upon a secure and solid platform and be fitted with an isolator switch.

# Offshore Powerboat Rules 2018

The battery isolator switch position must be clearly marked for safety reasons.

## 3.23 - ELECTRICAL HARNESS

Electrical Harness:

Properly protected terminal boards shall be used with flexible (not solid core) cabling supported well up to the terminals and at suitable intervals throughout the length of any run. Where relative movement or vibration occurs across any gap, cables shall be sheathed in plastic or metal tube anchored at both ends.

Reinforced cable suitable for marine duty shall be used. Electrical equipment in engine compartments shall be kept to a minimum and sited away from heat or fuel.

## 3.24 - FIRE PREVENTION

All boats with inboard engines shall carry a fixed automatic fire extinguishing system.

This system shall be properly installed, engineered and maintained.

Sensors and injectors shall be in danger regions of the engine compartment/s.

ALL boats including inboards shall carry a minimum of 2 fully charged dry powder 2kg fire extinguishers.

## 3.25 - RETIREMENT FLAG

All boats shall carry an orange rectangular flag of minimum size 60cm X 40cm as a means of announcing retirement from a leg of the race, and the means of maintaining it aloft in a well visible position.

## 3.26 - NATIONAL FLAG

All boats shall display their national flag in either fabric or graphic form so as to be visible from both sides of the craft.

## 4 - ENGINES

### 4.1 - ENGINE LIMITS

Engine volume limits are as described in the Class classifications for naturally aspirated engines.

# Offshore Powerboat Rules 2018

Where engines include a supercharging or Turbocharging forced induction system (or similar) then the normal cylinder capacity is to be multiplied by 1.4 and the boat will pass into the class corresponding to the volume resulting from this multiplication.

## 4.2 - ENGINE DESCRIPTIONS

ENGINE DESCRIPTIONS for all classes except A, B & C Note: The term 'OEM' = Original Engine Manufacturer

a) Engines eligible for Marathon classes other than the classes A, B and C (all these are 'free') must be based on currently, or previously advertised/available mainstream production units (ie, Mercruiser, Volvo, OMC, Yanmar, Cummins, FPT etc) available to the general public through normal distribution channels with a minimum production run of 500 units. Engines should be from the pleasure line, not the race line of engines from any manufacturer. If the engine is listed with separate runner exhaust, as described in section (g), the motor is not acceptable.

As an example, the Merc 525hp motor used in P1, has CMI headers with runner lengths greater than 6" long, so these motors would not be permissible. The old Merc 500 carb, had GILL/Merc alloy manifolds with common plenum, they would be acceptable, as would STD Merc 496HO's, or 502efi's.

b) The original bore & stroke must be retained, however, an allowance will be made for OEM piston oversize, i.e., +0.030.

c) Inlet system (intake manifold, intercooler, throttle body etc) MUST remain OEM; throttle bodies must retain OEM internal dimensions at butterfly. Turbochargers must be OEM as specified as STD for motor to which it's fitted.

d) Internal engine modifications are allowed, such as camshaft choice, cylinder head gas flowing etc, however, rules for OEM induction systems, intercoolers, turbochargers, etc will be strictly enforced.

e) Engine components may be compared to standard OEM components to establish eligibility.

f) Transom or side exhausts are permitted, providing that the total volume of water passing through the engine cooling system mixes with the exhaust gases from the manifold.

g) Choice of exhaust manifold is free, so long as they are not of the separate runner type, i.e., Stellings, CMI, PF Marine, KE Lightning, etc. Exhaust ports must exit to a common plenum within the manifold, with each branch not exceeding 150mm in length. It is in the competitors' interest to contact the authority prior to purchasing exhaust systems to ensure eligibility if there is any doubt. No dispensation will be given for exhaust manifolds that do not meet the rules.

# Offshore Powerboat Rules 2018

- h) Drive systems are “free”.
- i) Height adjustable hydraulic engine jacks are NOT permitted.

## 4.3

Single engined craft and single shaft driven boats may compete. All such boats MUST carry an auxiliary engine for use in emergency breakdowns capable of propelling the craft at 5mph for a minimum of 35 miles. This engine can be stowed in the boat but must be shown to be sufficiently retained so that it will not work loose or cause damage.

## 4.4

Boats with more than one outboard engine or outdrive must have a properly engineered tie-bar system

## 4.5 - ENGINE MOUNTINGS

Outboards: Engine mountings shall be attached to the transom with at least 4 bolts, which shall be either pinned or lock-nutted.

Inboards: Engine mountings shall be sound and the mounting bolts securing to the hull shall be pinned or lock-nutted.

Engines shall be enclosed in their own compartment/s with the exception of normal ventilation. Each and every engine need not be contained in its own compartment. The compartments shall have rigid covers.

# Offshore Powerboat Rules 2018

## 5 - SAFETY EQUIPMENT FOR THE BOAT

### 5.1 – EMERGENCIES

It must be understood that in Marathon Racing every team/crew must be prepared for any/all eventualities with regard to their own safety.

If any team experiences difficulties or an emergency during the race, then they must be aware and prepared to be effectively on their own until rescue teams (from whatever source) can arrive.

Every team should have emergency plans which should include a risk assessment setting out how they would deal with an on board emergency situation until such time as rescue teams arrive.

Racing is inherently dangerous and crews must be prepared to ensure their own safety at all times. In the event of an emergency crew cannot rely on race organisers to facilitate a rescue and must be self-sufficient and capable of initiating their own rescue.

### 5.2 - INFLATABLE LIFE RAFT

It is mandatory that every boat carries one certified and in date Inflatable Life Raft capable of carrying the complete crew, together with adequate fresh water and a first-aid kit.

The life raft shall be stowed in an easily accessible position so that in the event of emergency the life raft can be quickly released, particularly in the event of fire.

Aviation style life rafts are not acceptable for Ultra Marathon races where the total race length exceeds 500nm.

The life raft is recommended to comply with ISAF Offshore Special Regulations (OSR) Appendix A Part 2 or ISO 9650 Type 1 Part 1, Group A standard, and should be fitted with a boarding ramp. (Or equivalent NA approved standards)

### 5.3 – FLARES

All boats will carry an offshore standard flare pack comprising: 4 red parachute rockets, 4 red hand held, 2 orange smoke hand held, and 4 white collision hand held flares in a waterproof canister. If any flares are deployed must be replaced before the start of the next race.

# Offshore Powerboat Rules 2018

## 5.4 - VHF RADIO

All boats will carry a shipboard installation of a fully synthesized VHF transceiver with GMDSS capability carrying all 55 International Channels, with an aerial output of not less than 25 watts.

*Organisers may require additional channel(s) to be programmed into this radio to comply with local regulations.* In addition, a multi-channel waterproof hand held VHF radio, which should be carried by one crew member at all times when racing.

Each vessel must have a Ship's Licence for their radio equipment and at least one crew must have an Operator's Licence, both Licences to be checked at Race Control and be available for scrutineering.

## 5.5 - E.P.I.R.B.

A portable emergency position indicating radio beacon (E.P.I.R.B.) operating on 406 MHz shall be carried and stowed in an easily accessible position.

## 5.6 - TRACKER SYSTEM

Some long distance Marathon Races may require tracker systems, this decision will be made by the organisers prior to the event and details provided in the Advance Race Instructions.

For all Ultra Marathon Races a tracker system as defined by the race organiser is mandatory.

## 5.6 - FIRST AID KIT

All boats shall carry an adequate medical kit and emergency thermal protective aids for each member of the crew.

## 5.8 - RADAR REFLECTOR

All boats must carry a RADAR reflector suitable for the boat or a Search & Rescue Transponder - SART.

# Offshore Powerboat Rules 2018

## 6 - SAFETY EQUIPMENT FOR EACH CREW

### 6.1 - CRASH HELMETS

Crash helmets are to be worn by all persons on board at any time when the boat is on the plane during the journey to and from the race and during racing and practicing or testing.

Crash helmets may be removed when undertaking repairs providing the boat is off the plane.

Crash helmets must be orange in colour and have temple protection.

Helmets must have the boat's race number on the top, painted in black and minimum height of 6.5cm.

Helmets must be devoid of dents or splits and any modifications must not infringe on the standard.

Bolts used for fittings and attachment of a visor must be small and must not protrude into the inner surface of the helmet.

Visors must be devoid of cracks and easily detachable (i.e. not bolted down). Chin straps must be in good condition and operative.

All crash helmets must comply with the following specifications:

It is mandatory to wear a helmet made to a minimum standard to include UN ECE22-05 or Snell M2000 or M2005 or Snell SA2000 or SA2005.

It is also recommended that helmets are replaced when they reach five years old from date of manufacture.

When a helmet is produced for scrutineering the wearer must prove compliance with the above standards; this may be by means of product literature.

# Offshore Powerboat Rules 2018

## 6.2 - RACING VESTS

All crew must wear a racing vest suitable for their class.

Specification: All vests must have inherent buoyancy of at least 150 Newton and be fitted with collars.

The Grabner & Hutch Wilco Motorboat Offshore Racing vests are also acceptable.

Lacing ties and/or straps must be adequate & in good condition.

All straps must be at least 40mm wide & have a minimum breaking strain of 500kg.

There must be lifting straps at the front or on the shoulders.

Zips are not permitted as the sole means of fastening a racing vest.

Where zips are used as an ancillary means of closure, they must be in working order.

Tears/rents or bad repairs through which buoyancy material may leak out are not permitted.

Vests shall be at least 60% orange or yellow or have orange or yellow panels.

Vests must have a lifting eye or strap attached to the main harness.

Vests must not be able to ride up over the wearer's head & be secure to wearer's body.

The disposition of the solid buoyancy must be such as to ensure that an unconscious person shall float face up in the water.

The vest must have impact protection material covering the back.

For crew using restraints, the racing vest must comply with cockpit rules

The use of an automatically inflatable racing vest is prohibited these must be manual.



# Offshore Powerboat Rules 2018

## **6.3 - EFFICIENCY OF CRASH HELMET & RACING VEST**

The efficiency of the crash helmet and racing vest is the sole responsibility of the wearer.

## **6.4 - PROTECTIVE CLOTHING**

All crew members whilst racing must wear suitable protective clothing that covers the torso and all limbs to the wrists and ankles.

Protective clothing used must be durable enough to provide bodily protection and is recommended to be fire retardant.

## **6.5 - MEDICAL COMPRESS & KNIFE**

All crew must carry on their person whilst racing a whistle, medical compress and a knife.

# Offshore Powerboat Rules 2018

## 6.6 - SAFETY EQUIPMENT

All Boats must start and finish and carry throughout the race the following minimum equipment:

### ITEMS OF EQUIPMENT

3.4 & 3.5 Carbon monoxide sensors and/or alarms for boat with full or partial canopies

3.10 Certified Lifting strops, if specifically required by Race Instructions

3.7 Manual Bilge Pump

3.13 Hand or foot Pump (RIBS only)

3.15 Tow Rope and Warps

3.16 Anchor and rope/chain minimum length 50 metres.

Additionally in Ultra marathon races all boats must carry a sea anchor of suitable size for their particular vessel.

3.19 Fog horn and Torch

3.24 Two 2kg dry powder fire extinguishers suitably mounted close to engines or crew

3.25 Orange retirement Flag 0.60m x 0.40m

4.3 Auxiliary engine (single engine race craft only)

5.2 Inflatable Liferaft compliant with rules.

5.3 Flares:-4 x red rocket. 4 x red handheld.

2 x orange smoke. 4 x white collision

1 mini flare pack to be carried per person

5.4 VHF handheld radio

5.5 1 x E.P.I.R.B operation on 406MHz.

5.6 Tracker System as specified or provided by event organiser

5.6 Adequate first aid kit. 1 x foil blanket + thermal protective aids for each crew member.

5.8 Radar Reflector

6.1 Crash Helmets compliant with rules

6.2 Life jackets compliant with rules

6.4 Adequate protective clothing

6.5 Whistle, Knife, Compress

6.6 Emergency Grab Bag: An Emergency Grab must be carried on board at all times to include fresh water, day and night flares, Thermal Protective Aids, First Aid Kit.

Recommended to include portable VHF Radio, passports, money and credit cards.

6.6 Satellite Telephone. For all Ultra Marathon Races an operational satellite telephone must be carried on board at all times.

Also the following safety equipment must be attached to the Boat.

6.8 Fixed VHF transceiver with GMDSS capability, as per rule.

6.9 GPS & RADAR equipment as per Racing Instructions.

# Offshore Powerboat Rules 2018

## 7 - TURN MARKS AND MISSED MARKS

### Turn Marks

All marks, including GPS marks will be laid in the approximate positions referred to in the race instructions.

It is the competitor's responsibility to follow the geographical course as described in race instructions and to drive close enough to the course marks to ensure that they have been seen to pass them correctly.

Competitors who do not follow the geographical course as described in race instructions will be disqualified from the race or in the event of multi-leg events they will be disqualified from that leg of the race.

The Event Race Instructions will specify the colour and shape of the course marks. Should a mark go missing the driver must pass through the approximate geographical position referred to in race instructions.

If a boat hits a turn mark – A Yellow Card penalty or other penalties may be applied.

If a boat destroys a buoy or cuts the securing line – A Yellow Card may be applied.

If a boat damages a buoy a fine will be applied.

### Missed Marks

A competitor may NOT return to a missed mark.

The competitor must continue to follow the designated course passing all remaining marks correctly.

Dangerous driving resulting from missing marks will result in further penalties.

The missed mark penalty is as described in race instructions.

# Offshore Powerboat Rules 2018

## 9 - ULTRA MARATHON RACES

Ultra marathon races are defined as races where the total *distance of* one or more *race legs* exceeds 500 nautical miles.

Alternative Starting Procedure: For Ultra Marathon Races standing or near standing starts will be permitted instead of the procedure defined in Rule 305. Full details of this procedure will be included in race instructions and described at drivers briefing. Safety requirements for organisers of Ultra Marathon Races

- . The organisers must establish a central race control location from which a dedicated team can monitor the progress of boats via the boat tracker system.
- . The organizer must also present risk assessment and an environmental risk assessment to the National Authority at least 90 days prior to the start of the race.
- . The organisers must clearly define the financial implications applicable in the different countries should a boat have to be rescued or team members have to be transported ashore by rescue services.

For all Ultra Marathon Races a tracker system as defined by the race organizer is mandatory. Satellite Telephone. For all Ultra Marathon Races an operational satellite telephone must be carried on board at all times.

For Ultra Marathon races, the organisers must provide full details of the race entry fees and conditions.

# Offshore Powerboat Rules 2018

## 10 World Offshore Championship Series rules

The Championship is a series of races, for 2018 it is four namely Poole, Falmouth, Cowes/Torquay and Torquay/Cowes.

These events are run by the organisers Moto2017 for the Poole event, IPRC for the Falmouth event and the BPRC for the Cowes Torquay and Torquay Cowes races.

All four races are equally weighted in terms of points there is no need to score points in all races. The points will be allocated to the driver and co-driver and a team may change their boat during the championship once only, but may not return to first boat without freezing the point they have accrued at that stage. They will then accrue point as a new entry, but will not have to pay second entry fee. Points will be awarded to race on the basis of the popular 25% reducing system namely: 400, 300, 225, 169, 127 etc...

Should there be a tie in terms of the championship winner then the team who have scored most points in the longest race (in terms of actual mileage raced) will win, then if this doesn't see a winner the second longest race will be taken etc...all other ties will be left tied.

There is only one way to enter the championship and even if one or two races have taken place the entry fee remains the same, there is no refund if a boat cannot continue in the championship after one or two races.

If a team is disqualified for a 'premeditated' serious technical infringement, such as being found to be underweight or to be using illegal fuel then the boat will lose all the points accrued to date, but the team may continue in the championship and can continue to score points in subsequent races. Other disqualifications where seen to be 'accidental' or not major performance increasing matters then they will only be disqualified from the race in question.

The rules define a number of classes and these will be demoted in the race and championship results, but there will only be one main championship winner who will be team that score most points in the overall finishing order in each race. This team will be crowned the World Offshore Champions. However there will also be a champion in each sub class but they will only be considered as a world champion if there are at least 5 boats racing in that class in the championship.

# Offshore Powerboat Rules 2018

## Appendix 1 - CREW SAFETY (508)

All these Rules apply to all categories of Marathon boats with restraints.

These rules are important safety rules and require adhering to, however certain rules may be slightly varied where a team feel there is a better safer alternative solution. If this is the case then a team should write to the Championship committee with the fullest written details and drawings.

If the committee in conjunction with the scrutineering team concurs to this requested variation then they may be accepted for use in the championship.

All the crew, who must be seated, must have a restraint system comprising of and conforming to the following rules:

A Reinforced Cockpit with Canopies is defined as a containment area for crew and can be constructed as an integral part of the boat. This Reinforced Cockpit Area must be designed and constructed to a specification capable of withstanding the forces of a water impact when running at the highest design speed of the boat, and therefore protecting all members of the crew in the event of an accident. The various components that constitute the Reinforced Cockpit shall be properly maintained to ensure reliable operation of all components, with emphasis being placed on the canopy release mechanism, emergency air supply and restraint systems.

These rules also apply to any boat in any class using Reinforced Cockpits with Canopies.

### 508.01 - COCKPIT EVACUATION / IMMERSION TRAINING

Before racing in a craft with restraint systems, all crews must have passed in the last fourteen months, an immersion training in a restraint system to ensure that they can exit a reinforced cockpit crew compartment successfully.

Prior to taking the Immersion training, all crews must have a valid scuba certificate or have received suitable training. This alternative training should be approved by The National Authority.

An immersion Certificate to certify the passed test, showing the expiry date, must be delivered by Experts recognised by a National Authority.

All riding crew members using restraints must sign the National indemnity form prior to competing in any race or practice.

### 508.02 - DRAWINGS AND MEASUREMENT

Three view drawings (plan, side and elevation) of the design of the Reinforced Crew Cockpits(s), the Bulkheads, the type of Canopy, the Buoyancy System and the Restraint System anchorage Points must be lodged with UKOPRA of the measurer and verified at the time of craft measurement. Drawings shall be provided showing canopy aperture dimensions for full or partial canopies, single or tandem arrangements. Arrangements shall describe whether fore and aft, or side by side seating is fitted.

Drawings shall show the method and construction of release devices.

# Offshore Powerboat Rules 2018

Drawings should show the material specification of the transparent areas.

Prior to Boat Measurement the drawing and material specifications shall be sent to the Measurer requested to measure the boat. On completion of measurement, the drawings and material specifications called for by the designer shall be lodged with the measurers National Authority before they issue a certificate of compliance and measurement.

## 508.03 - REINFORCED COCKPIT AREA AND CANOPY

The reinforced cockpit(s) shall be of a closed type design with a minimum of one opening hatch and constructed to a similar strength as the running surface of the boat. This area must be the sides, floor, decking and bulkheads fore and aft. All new cockpits built after 1<sup>st</sup> January 2019 shall be built by a UKOPRA registered cockpit builder.

Cockpit builders wishing to be registered must submit drawings and laminate specifications to UKOPRA – this process can take a month.

## 508.04

Canopies must be a composite structure with the following features.

## 508.05

Polycarbonate areas are strongly recommended to be as small as possible while still maintaining that the driver and co-driver have clear, safe and undisturbed visibility ahead at sea level whilst racing. It is strongly recommended that these polycarbonate areas are built using 12 mm thickness, or more.

The combined visibility of driver and co-driver must be through a horizontal arc of 22 degrees (112.5 degrees either side of the center line of the boat).

These polycarbonate panels are to be recessed into the composite structure and may be bonded using a suitable bonding agent, and/or “bobbins”. It is recommended that there is also a through bolted outer flange for the fitting of the polycarbonate panels.

## 508.06 - SCREEN FLANGES

Screen flanges shall be a minimum of 50 mm at forward direction and mm towards sides and should be fastened every 100 mm if using “bobbins”; it is recommended to use metal “bobbins” with heads, as opposed to the recessed plastic type.

The outer polycarbonate area of the flange fitting must not be painted, so that the measurer/ scrutineer may monitor any discrepancies.

- . Window to flanges joints must be glued and/or use bobbins of nylon or aluminium.
- . Bolts: min 6 mm stainless steel, nylock nuts, washers.
- . Bolt spacing: max. 10 cm if not glued
- . The outer edges of the canopy surrounding the hatch, must be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsize.

## 508.07 - ROLL BAR

# Offshore Powerboat Rules 2018

These Restraint Cockpits must be fitted with an internal roll bar, two in a tandem cockpit as a minimum. There must also be, between the two single cockpits, an anti-compression strut or structure of similar strength to the roll bar.

- b. Roll bar in front of/around each crew member.
- c. Roll bar strong enough and well secured to the bottom stringers.
- d. Central compression strut to hold roll bar, for side by side cockpits. Side compression struts may also be necessary for side by side cockpits.
- e. Alternatively, instead of a compression strut, the design of the cockpit primary structure will consist of a center roof rib connected to the roll bar and the aft bulkhead with sufficient strength to satisfactorily react the design impact loads.

## 508.08 - HATCHES

Hatch openings shall have a minimum of 25 mm flange.<sup>[SEP]</sup> Hatches must have a slot for pry bar, on the opposite side of the hinges, use in emergency/rescue.

## 508.09 - HATCHES

Hatches should be recessed on the front and sides.

The outer edges of the canopy surrounding the hatch, should be fitted with a water deflector, (height 10 mm min) to prevent water forcing open the hatch in the event of a capsize.

Water deflector to be fitted only on front and sides of hatch, not behind of hatch. (A water deflector on back of hatch might force water into cockpit area.)

## 508.10

It is mandatory that the hatches are constructed to the same specification as the cockpit. The hatches shall be fitted with a catch which has a positive open and positive close mechanism and should hold the hatch against lateral forces. These hatches shall be able to be opened from both inside and outside the cockpit and must have a second emergency mechanism to allow the rescue team to easily remove the hatch from outside if necessary.

These hatches should be fitted with hinges with short release pins. This is important because long pins invariably bind the hinge.

## 508.11

There should be one or more divers grab handles fitted to the outside of each hatch.

## 508.12

Canopy hatch release handles, which must be provided both inside and out, must be painted fluorescent orange or have a fluorescent orange background panel to identify them and directional arrows to indicate the method of opening.

## 508.13

The canopy lid hinges and the canopy hatch covers release mechanism must not encroach within the canopy aperture area, and these hinges and release mechanisms must not in any way hinder the exiting of crew members when fully race fitted.



# Offshore Powerboat Rules 2018

508.14

Canopy openings should have the entry/exit apertures located directly above the crews' heads. The canopy aperture openings should be at least 0.55 m in length and 0.55 m in width. If the crew is seated side by side, then the opening should be at least 0.55 m x 0.825 m wide. In tandem configuration, the opening(s) should be 0.55 m x 0.55 m per crew member. The canopy apertures should be cut with all corners having a radius of 0.025 m minimum or 0.25 m maximum. The radius should be constant and have a smooth finish to relieve stress.

508.15

The canopy aperture must have a 20 mm wide (minimum) fluorescent orange band around the opening, both inside and outside of the opening.

508.16

It is mandatory that one single air supply (not oxygen) and a bottle will be provided for each riding crew member. The air supply must be securely fixed adjacent to, or on each one of them. It is recommended that sufficient air be provided in each individual bottle for ten minutes. Air bottles must have a pressure gauge fitted for visual checking at pre-race scrutineering. This gauge should be filled with liquid and be at least 5 cm in diameter for easy reading.

Air supply bottles shall be "Turned On" before leaving the dock to go to starting a race or taking part in practice and/or testing.

508.17

Reinforced Cockpits must have flood tubes or other means of flooding the cockpit to equalise the pressure quickly in an accident. The floor of the cockpit should be as air tight as possible to help the cockpit pressure equalise far more quickly when in an upturned position.

508.18

Boats with restraints must have stop buttons/switches located in the cockpit area, immediately accessible to driver, co-driver and rescue officers. The stop buttons/switches must be identified by a fluorescent colour.

These switches must shut off all fuel pumps as well as the ignition circuit.

In the case of diesel boats, the stop control cable for the fuel injection pump shall be a non-sleeved cable, so as to eliminate the cable being able to bond in a fire.

508.19 - STROBE LIGHT

All boats shall have a White or Orange High Intensity Strobe Light fitted to indicate "coming off the plane" but not needing assistance. The strobe light must be able to be operated by the throttle man, and should be operated by the throttle man if a problem occurs, to enable any following race boats to take avoiding action. The strobe light shall be mounted on the top rear of the canopy. When duel canopies are used, the light may be on or behind either one.

This strobe light may also be used as a substitute for the orange retirement flag when returning to port under reduced power.

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## 508.20

Cockpits with Restraints must be fitted with rear of head protection for each crew member. This must be an integral part of the seat, which must be attached directly to the structure of the Restraint Compartment. The head protection must be a minimum of 0.2m wide and extend at least 75% of the height of the safety helmet as worn by the crew whilst in the normal seating position. There must be a minimum safe vertical and lateral clearance between the canopy and each of the crew members when in the normal seating position.

## 508.21

The Restraint System must consist of a 5 or 6 strap harness and should utilise a 75 mm lap belt, a 50

mm strap over the shoulder harness rated at 4,100kg (9,000 lb.) and grommited to prevent chafing or cutting of the belt. Harness straps must be attached directly to the cockpit structure. Those straps close behind the driver's head and neck must be 100 mm to 150 mm apart at point of attachment.

The shoulder harness should be installed at 90 degrees to the spine at shoulder line to minimise compression injuries under high "G" loading. All straps must be free to run through intermediate loops or clamps/buckles. All anchor point bolts must be fitted with backing plates of 10cm minimum width.

The driver harness attachment bolts in reinforced cockpits must consist of minimum grade EN8 bolts, with an 8 x 1.25 mm thread and locked nuts. There must be a spacer and plain washers on each bolt. The spacers must be glued to the cockpit structure. Intention of these spacers is to prevent buckling of surface material near bolts. This always leads to local delamination which easily spreads out over cockpit structure, when it is under stress.

On the sides of the structure, which has to take up the force on the attachment bolts, there must be a stainless steel plate (washer of minimum 3mm thickness and 100 cm<sup>2</sup> area).

When using seats with suspension, and therefore not using a bulkhead restraint anchorage, drawings must be lodged with UKOPRA of the measurer and approved prior to boat measurement. All restraint systems must have a common method of release. The single lever method (sometimes called the NASCAR type) or rotary type, are both acceptable restraint release systems.

Both types of restraint release must be examined for satisfactory operation by the scrutineer before every race.

The harness system must comply with Drawing below.

The shoulder harness should be installed 90 degrees to the spine at shoulder line to minimise compression injuries and the high "G" loading

Lap belt should continue in straight line to anchorage.

## 508.22

A quick release steering wheel may be fitted on a boat with personal restraints, but all drivers must be able to exit the cockpit without removing the steering wheel.

## 508.23

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Rear view mirrors are mandatory, as well as a method of cleaning the canopy whilst under way.<sup>[SEP]</sup> Each wing mirror must have a minimum size of 60 sq.cm and be bolted on 2 points to assure proper mounting.

508.24

Two fire extinguishers, each a minimum of 2kg, or of equivalent capacity, must be carried and be readily accessible to the crew.

All crew containment areas of inboard engine 508 canopied boats must be fitted with a carbon-monoxide sensor and/or alarm.

508.25

Racing Vests - the efficiency of the racing vest is a matter of the exclusive responsibility of the wearer. Every crew member whilst on board must wear a racing vest during the practice runs and throughout the race. Racing vests must be coloured high visibility orange or yellow. The racing vest should have epaulets/handles to help extract crew from the boat.

The racing vest must have crutch straps or a method of ensuring that the vest does not "ride up".

***The use of an automatically inflatable racing vest is prohibited these must be manual.***

508.26

Each Reinforced Cockpit Area shall have one or more water activated light(s) or similar.

508.27

All boats with restraints must have their bows painted fluorescent orange for at least 0.5 m. Only boats with restraints and closed canopies are allowed to use orange coloured bows. If the hull is of a similar colour, then there must be a white separating band of at least 0.15 m wide to ensure that the fluorescent orange band is obvious.

The number of riding crew members must be written in black (so they read correctly if the boat is capsized) in at least 0.25 m high numbering on the orange nose in the best of the following three locations where the shape of the boat allows:

- a) The lower running surface.
- b) The topside of the hull.
- c) The deck of the hull

Should any boat be found to have contravened the riding crew number requirement, the penalty shall be disqualification from that event.

Note: Numbers shall be placed on the orange nose, not the white band area

